

## EXECUTIVE SUMMARY

The Town of Eagle has engaged WALKER Consultants to complete a *Downtown Parking Study* (or the “Plan” for the purposes of this document).

The goals of this Plan are to evaluate current supply and demand conditions within the downtown core, bounded by Grand Avenue to the north and west, Capitol Street to the east, and 5<sup>th</sup> Street to the south. Further, WALKER was tasked with developing recommendations for parking policy and management in Eagle, using a review of existing off-street parking requirements and guidelines set forth in the Eagle Municipal Code, and incorporating industry best practices.

Based on the team’s findings and recommendations, Eagle’s leadership now has the exciting opportunity to make important, impactful changes for the Town. These changes will help to ensure that the downtown parking supply will continue to serve its users well; that existing regulations are effectively and equitably enforced; that any and all changes to parking regulations or enforcement of said regulations are well-communicated to the public; and that parking will not be a major financial or practical deterrent to downtown’s continued development.

Following are key findings from this study:

- **Supply Sufficiency:** Eagle’s existing downtown parking supply is more than sufficient to meet current demand, though there are a small number of localized shortages that can be alleviated by upping enforcement of existing regulations. The efficiency of current supply could be further improved by increased signage and, eventually, paving and striping of existing unimproved rights-of-way used for parking.
- **Public Right-of-Way:** The lack of enforcement of certain regulations, namely Section 11.10 of Eagle’s municipal code, which permits the impounding of stored and/or abandoned vehicles on public property, is seriously jeopardizing the integrity of the public rights-of-way in downtown Eagle.
- **Community Education:** Many community members are unaware of existing parking regulations, some of which are essential to the integrity of Eagle’s public rights-of-way. Targeted outreach and education on these regulations and the benefits they offer to the community is essential.
- **Private Off-Street Parking:** At present, many developers are deterred by existing off-street parking requirements in Eagle’s downtown. Those who still choose to develop frequently apply for variances from the off-street parking requirements, thereby undermining the existing regulations and taking up significant staff and elected official resources.

Following are key action items recommended by this study:

- **Enforcement of existing parking regulations.** The Town of Eagle has already adopted a number of excellent parking regulations that could offer great benefit to the community, but are not adequately enforced, primarily because of current staffing levels. These regulations include Section 11.10 of Eagle’s municipal code, as well as the two-hour parking restriction along Broadway. As such, WALKER

recommends that the Town consider hiring one additional staff member dedicated to parking/code enforcement (either part-time or full-time, depending on need as determined by impacted departments).

- **Demarcation of parking within unimproved rights-of-way.** WALKER recommends that the Town develop and install signage detailing how to park on all unimproved rights-of-way, and treating this instruction as an enforceable regulation. Eventually, WALKER recommends that the Town look towards paving and striping all unimproved rights-of-way used as parking.
- **Community Education plan.** WALKER recommends that the Town embark on a Community Education program in the first quarter of 2018 to provide information to community members about existing parking regulations and their benefits. The Community Development department is already in the process of implementing this recommendation.
- **Changes to off-street parking requirements.** WALKER recommends that updates to the Town’s land use code include revision of the off-street parking requirements to include increased opportunity for administrative reduction of requirements without undergoing a variance process. Specific recommendations include:
  - Consider adopting a uniform off-street parking requirement of 1.5 or 2 spaces per 1,000 square feet of new development in the Central Business Zone and Broadway Districts.
  - Maintain existing off-street parking requirements outside of these Districts, with a review every two years by the Planning Commission to determine whether and where to extend the uniform off-street parking requirement of 1.5 or 2 spaces per 1,000 square feet.
  - Include a separate off-street parking requirement for studio and “micro” multifamily residential units at 1 space per unit.
  - Amend Section 4.070.050 H to require a shared parking study performed by a third party experienced in producing such studies. Allow for up to a 25% reduction from the required off-street parking with approval from the Community Development Director; reductions higher than 25% should require approval from the Planning Commission.
  - Allow for a reduction of up to 10% with approval from the Community Development Director for multimodal infrastructure, such as bike share, fixed bike parking, fixed and covered bike parking, car share, etc.
  - After new parking requirements and reduction opportunities are in effect, review development impact annually for five years. This review should be performed by the Community Development Director, with report-outs to the Planning Commission and Town Board.
- **Near-term and mid to long-term funding allocations.** WALKER recommends that Eagle take near-term (1 to 2 years) action to fund the following recommendations discussed in this report:
  - One additional staff person to assist in regular enforcement of existing parking restrictions and other restrictions as added (e.g. demarcation of how to park within a certain block face).
  - Signage to demarcate how patrons should park within rights-of-way in the study area and show drivers where parking is available.

- Staffing and/or consultant time to create parking brochure and other communications with Eagle citizens.

In the medium term (5 to 7 years), WALKER recommends that Eagle take action to fund the following additional items:

- Paving and striping of unpaved rights-of-way dedicated to on-street parking.
- Handheld LPR units for enforcement staff.

This Plan is organized into five sections, as listed below. Each topic-based section focuses primarily on findings and recommendations pertaining to that particular topic. The Implementation section of the Plan focuses primarily on methods and considerations for “rolling out” changes to parking policy and framework.

#### *PLAN SECTIONS AND ORGANIZATION*

- I. Plan Context and Input
- II. Existing Supply and Demand
- III. Supply and Demand Management
- IV. Code and Policy
- V. Implementation



# 01 Plan Context and Input

## SECTION 1: PLAN CONTEXT AND INPUT

Eagle is a rural mountain community experiencing the growth occurring in many of Colorado’s small towns. The Town is currently in the process of evaluating several development opportunities, concentrated primarily in its 13-block downtown bounded by Grand Avenue, 5<sup>th</sup> Street, and Capitol Street. At present, parking in the downtown is largely unmanaged; Broadway, the Town’s main thoroughfare, has recently undergone expansive landscape and beautification updates, and now offers 2-hour time-limited parking. Other parking areas throughout the downtown are unrestricted and largely unimproved.

The Town is facing a number of issues common to small municipalities undergoing change. The public perceives a shortage of parking in the downtown core; additionally, several recent development projects have been unable to meet the Town’s off-street parking requirements. As a result, the Town is interested in not only understanding existing public and private parking conditions in its 13-block downtown core, but also evaluating current off-street parking requirements and identifying best practices for managing and operating the downtown parking supply. To help address these issues and determine a path forward, the Town hired WALKER Consultants (WALKER), a professional services firm specializing in parking and transportation planning, to help to develop a *Downtown Parking Study*. This limited-scope study is intended to provide analytical data on the existing supply and demand conditions in downtown Eagle, and provide recommendations on supply management, code and policy considerations, and implementation based on public input and best practices.

### KEY PLAN OBJECTIVES

The following figure (Figure 1.1) depicts the key objectives for each component of the Plan: Supply and Demand Management, Code and Policy, and Implementation.

Figure 1.1: Key Plan Objectives

<b>Supply and Demand Management</b>	<b>Code and Policy</b>	<b>Implementation</b>
<ul style="list-style-type: none"> <li>• Evaluate sufficiency of existing inventory to meet parking demand               <ul style="list-style-type: none"> <li>• Identify opportunities to increase efficiency and usability of parking inventory</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>• Ensure the Town's code leverages its ability to use its own parking resources for public purposes.</li> <li>• Ensure off-street parking requirements do not greatly hinder development</li> </ul>	<ul style="list-style-type: none"> <li>• Include public in decision-making</li> <li>• Ensure parking system changes are long-lasting and successful               <ul style="list-style-type: none"> <li>• Identify opportunities and methods to fund parking system updates</li> </ul> </li> </ul>

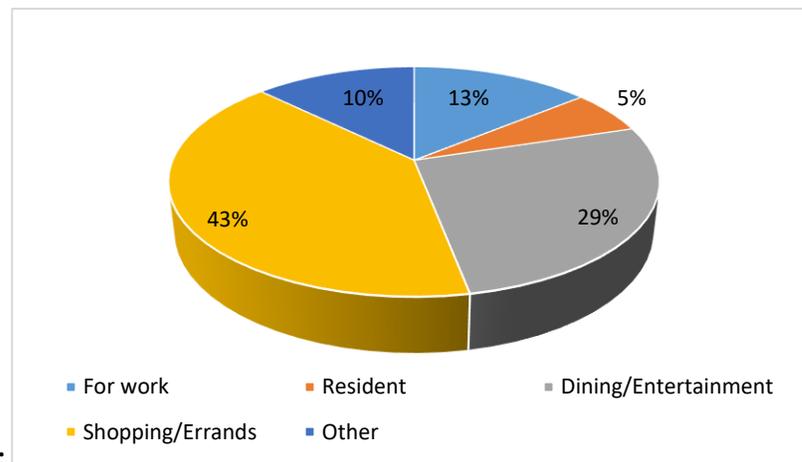
## PUBLIC INPUT OPPORTUNITIES

This study included public input opportunities in the form of a digital survey, which accepted responses from the public for a 3-week period in October. The survey generated a significant level of participation, with 368 total responses collected. Nearly all participants were residents of Eagle. Especially considering Eagle’s population size (roughly 7,000), this is a very impressive level of participation, and indicates the importance of this topic to the community.

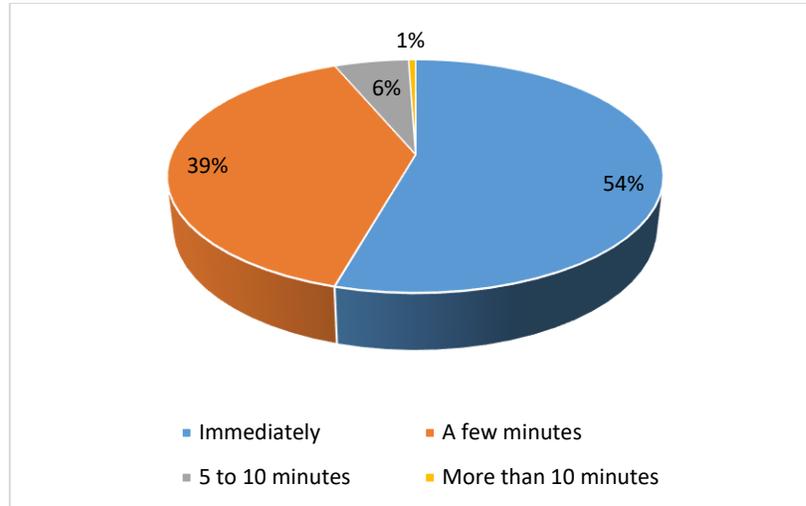
The survey focused on survey takers’ experiences with parking in Eagle, and their opinion of various best practice parking management techniques. This initial community feedback indicates that the supply of parking in downtown Eagle is convenient and sufficient for the overwhelming majority of users. While most respondents are interested in light-handed management techniques to make parking easier to locate and restrictions easier to understand, participants indicated a mild wariness of more heavy-handed management techniques common in larger communities. Survey results for the specific questions asked are shown below.

WALKER recommends that the Town engage in additional public input and community outreach prior to making definitive decisions about what parking management techniques and policies to implement; in fact, the Town is already developing a public outreach strategy on this topic for 2018. For those parking management and policy practices that are currently showing low support from the public but are still, in our professional opinion, a good option for Eagle, WALKER has provided additional detail on ways to curry public buy-in.

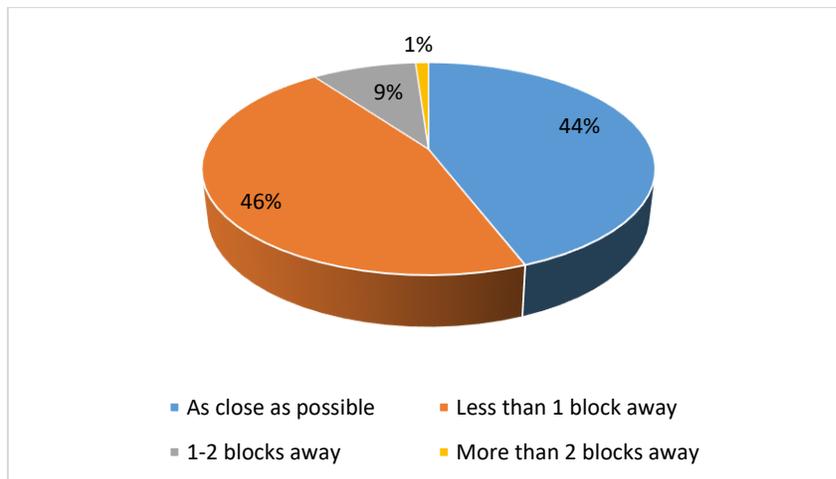
- **Why do respondents come to the study area?** Most respondents (72%) visit the study area to dine, shop, or run errands.



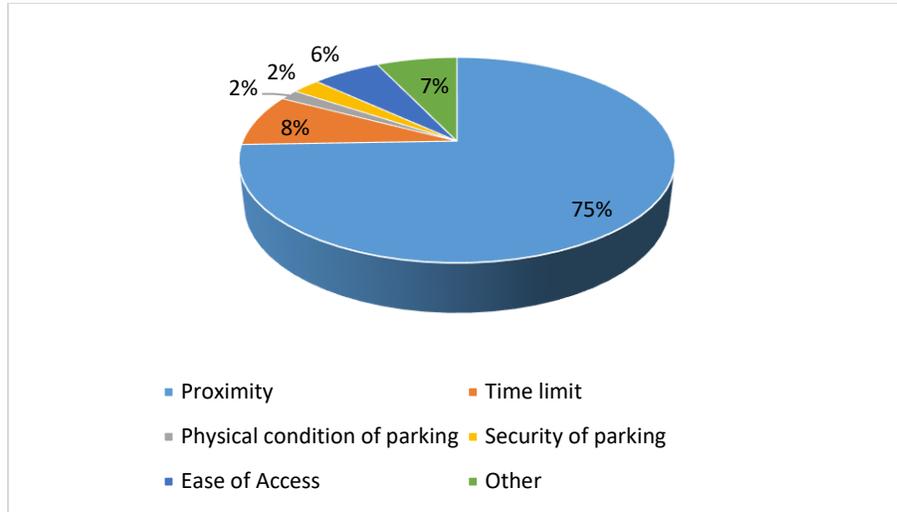
- **How long does it usually take to find parking in the study area?** 54% of respondents stated that parking was easy to find in the study area—most (94%) are able to find parking immediately or within a few minutes.



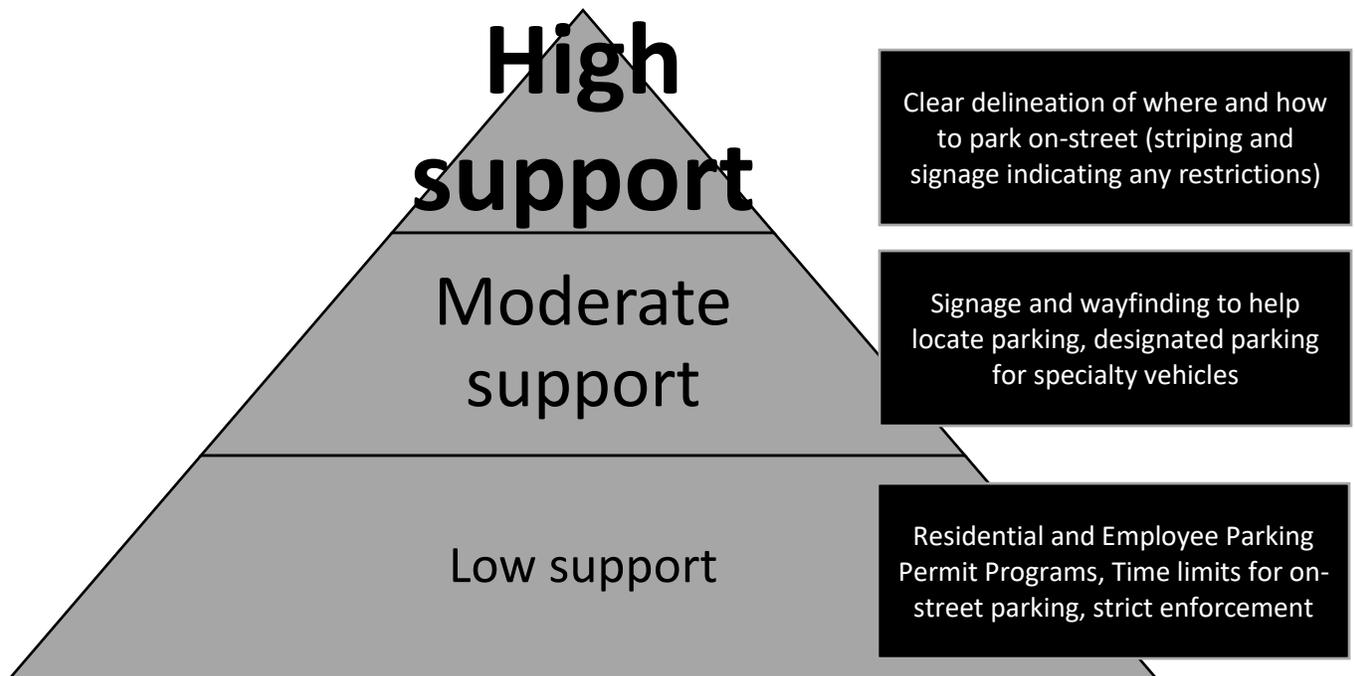
- How close to their destination do respondents wish to park?** 90% of respondents stated they prefer to park less than one block away from their destination. In denser communities where it is challenging to find very close parking options or multimodal opportunities have been prioritized, more people are used to parking far away, and their preferences for parking begin to shift.



- What is the biggest factor for respondents in deciding where to park?** Most respondents (75%) cite proximity to destination as the most important factor in determining where to park. Note that in communities with paid parking, many cite cost as the most important factor.



- Parking Best Practices:** Respondents were asked to rate their level of support for a number of different parking best practices well-suited for the Eagle community. Those with the highest and lowest support are listed below.



A detailed summary of survey responses is included as an appendix to this report.



# 02 Existing Supply and Demand

## SECTION 2: EXISTING PARKING SUPPLY/DEMAND

This section includes a summary of inventory and occupancy data collection and analysis for the Town of Eagle Downtown Parking Study. This analysis consists of a detailed survey of parking inventory, followed by a detailed survey of parking occupancies taken at key times. For the purposes of this study, parking inventories were collected on Tuesday, August 29, 2017; occupancies were taken on Tuesday, August 29 at 8 AM and 1 PM; Thursday, August 31 at 9 PM; and Saturday, September 2 at 1 PM. The occupancies were intended to capture typical peak weekday conditions, typical overnight conditions, and typical peak weekend conditions. Times were selected in conjunction with Town of Eagle staff to ensure peak activity was observed.

Parking infrastructure should be designed to accommodate the highest level of activity on a typical day, rather than activity that only occurs during certain times per year (such as a special event). As such, the Town chose to observe activity during the days and times above to reflect not only the overall highest-demand conditions for the mix of uses in the study area, but also during the typical peak times for various individual land uses (such as downtown retail, commercial buildings, and residences).

### KEY FINDINGS

The supply and demand analysis indicates that while the parking supply in downtown Eagle is more than sufficient to meet existing demand, a number of changes could be made to current parking management practices to ameliorate localized shortages and provide a higher level of service to users. Following are key findings from the supply and demand analysis:

- **Parking Asset Distribution:** There are roughly 696 parking stalls in Eagle's downtown. The majority are public (79%); 21% are private.
- **Configuration of Parking Inventory within the Study Area:** A high percentage (43% of all public parking) is informal, located primarily on unpaved and un-delineated public right-of-way (note that all inventories for informal public parking are estimated based on typical parking stall dimensions). This has led to a number of consequences that limit the parking system's overall capacity, including:
  - Inconsistent parking format (some users parking parallel, others parking angled, and others parking 90 degree).
  - Usage of the public right-of-way as storage for seasonal or large vehicles (such as boats and trailers) or other vehicles that do not fit in residential driveways.
- **Overall Occupancy:** Overall occupancy in the study area peaked on Tuesday at 1 PM, with 53% of all parking full (including private) and 54% of publicly-available parking full. The next most occupied period observed was on Tuesday at 8 AM, when 41% of the total parking (including private) was full and 43% of the publicly-available parking was full. Occupancy was concentrated in public parking areas closest to popular destinations along Broadway; occupancy was also quite heavy near A.G. Roofing Company, which utilizes informal public street parking for both employee parking and vehicle storage. The current occupancy patterns may be influenced by vehicles associated with Red Canyon High School, which adds approximately 30 vehicles to the system on typical weekdays; however, such vehicles were not specifically identified during inventory/occupancy data collection.

- **Public Parking:** There are roughly 551 public parking spaces in the study area, including 58 time-limited spaces located along Broadway, 239 unrestricted formal (striped and/or delineated) parking spaces, and 254 unrestricted informal spaces located primarily on unpaved public right-of-way. Time-limited parking along Broadway was the most heavily-occupied type of public parking, with 76% of time-limited stalls occupied at the Tuesday 1 PM peak. Unrestricted informal parking was the least occupied public parking type, with 47% occupied at peak.

### STUDY AREA

The inventory and occupancy analysis included on-street and off-street public and private parking areas in Eagle's downtown core, a 13-block area including downtown Broadway and surrounding residential and commercial areas. Inventory and occupancy was determined on a block-by-block basis in order to establish local conditions, rather than merely on a study area-wide basis.

Typically, WALKER's data collection maps identify specific public and private facilities; however, due to the largely informal nature of Downtown Eagle's parking system, parking was identified on a block-by-block basis. The following figure (Figure 2.1) shows the study area in which data was collected.

Figure 2.1: Study Area

TOWN OF EAGLE PARKING STUDY  
EAGLE, COLORADO



STUDY AREA  
BOUNDARY



SITE PLAN

NORTH

Legend

⊗ Block Identification



SK-04  
23-7738-00

Note: Larger version of this map provided as an attachment.

### INVENTORY/OCCUPANCY ANALYSIS

The study area includes roughly 696 parking stalls in its system. Noting that much of the parking within the study area is informal (unpaved, unstriped, etc.), WALKER has utilized standard parking stall measurements to estimate inventory.

The following figure (Figure 2.2) shows a breakdown of stall inventory by type.

Figure 2.2: Parking Inventory by Stall Type

Stall Type	Inventory		% of Total System
Public	551		79%
Private	145		21%
<b>Grand Total</b>	<b>696</b>		<b>100%</b>

Note that the public inventory type includes some stalls, specifically in the Town Hall lot, allocated to specific user groups, such as City employees or emergency vehicles, and are not available to the general public.

There are 11 public ADA stalls located in the Study Area.

As noted in the figure above, there is an estimated 551 on-street public spaces located in the study area. While some of these spaces are paved, striped, and signed, others are largely informal, and are located in swaths of open area along streets with no curb and gutter. Figure 2.3 shows the locations of each type of on-street parking, as defined below:

**Unrestricted informal on-street parking:** Parking areas within the public right-of-way that are not delineated or demarcated (i.e. no striping, signage, etc.)

**Unrestricted formal on-street parking:** Parking stalls within the public right-of-way that are clearly delineated and demarcated (with striping and some signage) but do not have any restrictions.

**Time-limited on-street parking:** Parking stalls within the public right of way that are clearly delineated and demarcated (with striping and signage) and have time restrictions.

Figure 2.3: On-Street Parking Locations by Type



### OVERALL OCCUPANCY

Peak weekday occupancy for the study area was recorded at 1 PM during occupancy collection on Tuesday, August 29, 2017. The following figure (Figure 2.4) depicts overall occupancy on a block-by-block basis within the study area during the recorded peak conditions.

Figure 2.4: Peak Conditions (Weekday at 1 PM)—Overall Occupancy by Block



As depicted above, occupancy is primarily concentrated in public on-street stalls near popular destinations along Broadway, and near the A.G. Roofing Company headquarters (located at 332 Grand Avenue), which uses informal public right-of-way for both employee parking and vehicle storage.

The following figure (Figure 2.5) shows public stall occupancy only on a block-by-block basis. Because occupancy is higher in the Town’s public inventory than in its private inventory, occupancy on a block-by-block basis appears higher when only public facilities are analyzed. This is particularly apparent along Broadway and in adjacent blocks.

It is important to analyze private and public inventory separately, as in the vast majority of cases, municipalities have limited management control over private parking, and cannot address operational issues that may arise in private parking facilities, such as localized shortages or enforcement problems.

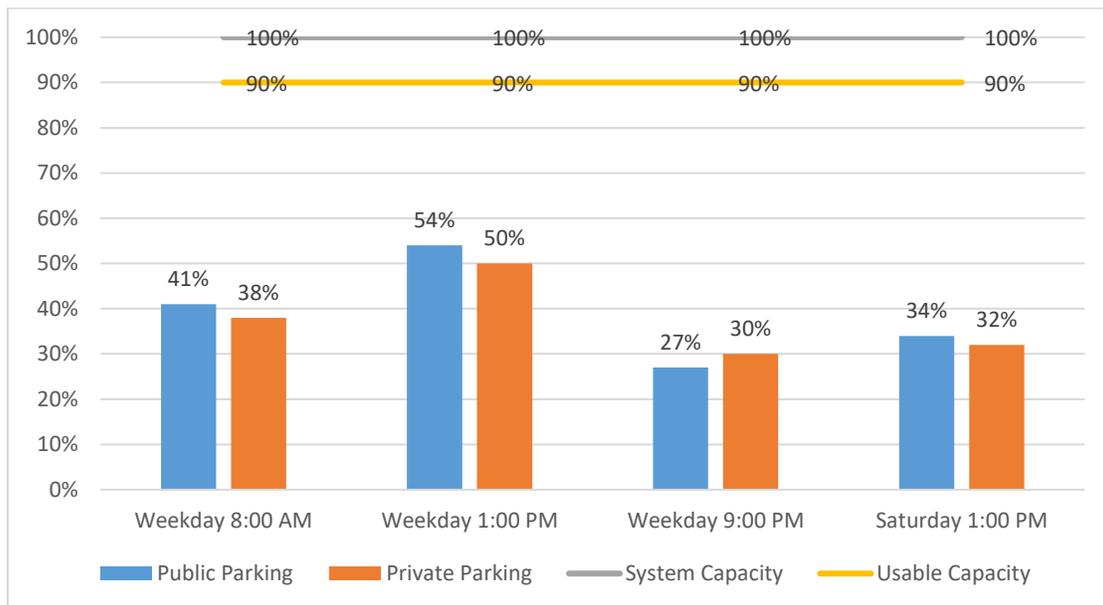
Figure 2.5: Peak Conditions (Weekday at 1PM)—Public Occupancy by Block



The following figure (Figure 2.6) shows the parking system occupancy (including both public and private parking inventory) by stall type, compared to usable capacity. **Usable capacity** refers to the maximum level of occupancy that can be reached prior to a negative consequence, particularly for first-time users of the system. Such consequences can include excessive circulation of an area to locate a parking space, traffic congestion, and leaving the system altogether, among others. It's important to consider usable capacity when designing a system, in order to avoid the consequences listed above, and preemptively plan for future infrastructure when these consequences start to occur.

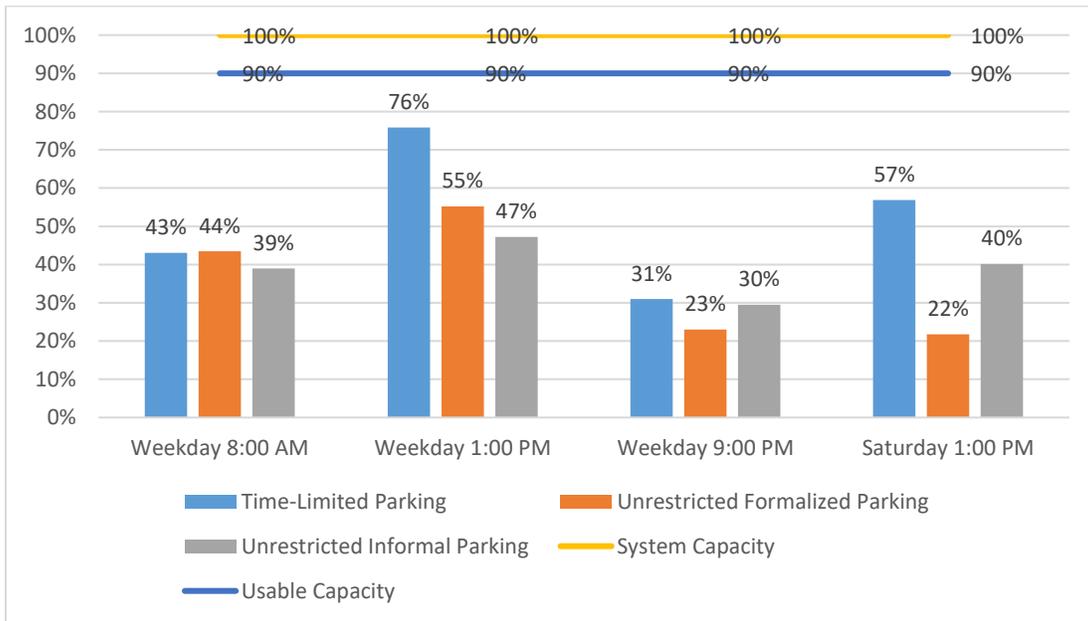
**System capacity** refers to the full capacity of the parking inventory (100%).

Figure 2.6: Peak Overall Occupancy v. Usable and System Capacity



The following figure (Figure 2.7) shows public parking occupancy by stall type (time-limited, unrestricted formalized, and unrestricted informal) compared to usable capacity and system capacity.

Figure 2.7: Peak Public Occupancy v. Usable and System Capacity



As shown, time-limited parking stalls, primarily located in the downtown’s most popular areas along Broadway, are the most occupied stall type.



# 03 Supply and Demand Management

### SECTION 3: SUPPLY AND DEMAND MANAGEMENT

The purpose of this section is to discuss potential methods to increase the efficiency and usability of Eagle’s downtown parking supply. While Eagle’s parking inventory is more than sufficient to meet current demand, implementation of some parking management techniques will mitigate community concerns, improve level of service, and help the parking supply accommodate densification in the downtown.

**Why parking management?** Parking inventory doesn’t have to be managed—but without management, various negative consequences can occur. Employees or other long-term parkers use high-demand parking, forcing customers to park farther away or even leave the system entirely. Supply is limited due to misparking or storage of personal vehicles. Localized shortages in frequently-visited areas, like Broadway, are exacerbated. When management practices—like clear delineation of where and how to park, and enforcement of restrictions—are applied, the parking inventory can efficiently and equitably serve all user groups.

**Parking practices don’t change overnight.** For the Town of Eagle, WALKER has set forth a series of recommendations meant to ease the community into a more traditionally managed parking system, without sacrificing the community’s unique character and small town feel. This section provides details on parking management techniques that focus on protecting the downtown parking supply as a community resource, and making that supply welcoming, efficient, and equitable for all users—from visitors to residents to employees. Section 5 of this Plan also includes recommendations on how to guide community outreach and participation as changes to the parking system are implemented.

**All parking management practices require enforcement (and staffing!) to be successful.** Presently, Eagle has a number of ordinances on its books that restrict how and where people park downtown—for example, 2-hour time restrictions on Broadway, as well as vehicle storage limitations on public rights-of-way. However, many community members, as indicated by this study’s survey results, are unaware of these restrictions. This is largely due to the community’s inability to enforce regulations with current funding allocation and staffing levels. In order for any new or existing parking management practice to serve its purpose—such as eliminating localized shortages, preventing excessive private use of public rights-of-way, saving the most convenient parking for visitors and customers, etc.—they must be enforced by trained and dedicated staff.

#### *CLEAR DELINEATION OF WHERE AND HOW TO PARK VEHICLES*

At present, significant portions of the public on-street parking within the study area is unrestricted and informal—located in unpaved and unmarked public right-of-way. This is resulting in several negative consequences, including:

- Reducing potential inventory due to parkers positioning vehicles inconsistently and/or inefficiently.
- Reducing potential inventory due to nearby residents and/or others using the public right-of-way for vehicle and other storage.
- Compromising the Town’s ability to perform community services in the public right-of-way, including parking, but also including development or repair of utilities, snow removal, etc.

As discussed in Section 1, the survey results suggested that Eagle’s community highly supports clearly delineating where and how to park vehicles, through paving, striping, signage, and shared information.

WALKER recommends the following action steps to achieve this well-supported parking management practice and mitigate the issues discussed above:

- Increase focused enforcement of Chapter 11.10 of the Municipal Code, which states that “any motor vehicle left on public property, including any portion of right-of-way, within the Town of Eagle for a period of twenty-four (24) hours or longer” can be impounded. Prior to increasing focused enforcement, provide public notice in the community newsletter. For example: “Did you know? The Town of Eagle’s Municipal Code prohibits leaving vehicles in the same public on-street parking space for a period of 24 hours or longer. The Town will be stepping up enforcement of this ordinance in coming weeks. If you have questions about this ordinance or how it applies to you, please contact xxx at (xxx) xxx-xxxx or [xxx@townofeagle.org](mailto:xxx@townofeagle.org).” Additional enforcement recommendations are discussed in the “Time Restrictions and Enforcement” section below.

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Figure 3.1: Code Enforcement Announcement Example—Fort Lupton, CO

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## Codes Enforced

To contact Code Enforcement directly you can call 720-466-6108.

### Abandoned / Inoperable Vehicles

Any vehicle on a street in Fort Lupton, as well as all vehicles on private property, must be currently registered, licensed, insured, and operable to be considered road-worthy. An inoperable vehicle is defined as

- Abandoned
- Discarded
- Unable to perform the function or purpose for which it was originally constructed
- Vehicle in a condition of being junked
- Wholly or partially dismantled
- Wrecked

Generally, an inoperable vehicle is owned and located on private property, and in order to be in compliance with the city code, such vehicles must be removed entirely from the property or completely enclosed in a garage. The police department, without notice, can immediately tow any vehicle that creates a nuisance.

### Outside Storage

The outdoor storage of items not normally stored outside also may be in violation of the city's code. For **example**: where the outside storage of bicycles, lawn furniture, grills, planters, etc. **may be allowed** in residential areas, the outside storage of scrap lumber, old tires, refuse, rubbish, old furniture etc. would be considered a violation.

### Parking

Fort Lupton enforces its own parking ordinances, as well as those in the State of Colorado's Model Traffic Code. Violations can include blocking public right-of-ways to parking in the wrong direction on a street. Additionally, everyone must obey special parking signs, such as designated handicapped parking. RV permits must be applied for unless parking is only for 72 hours.

### Snow / Ice Removal

Snow and/or ice must be removed from paved sidewalks within 24 hours following any fall of snow, sleet or freezing rain.

- Demarcate rights-of-way within the study area using striping and/or signage. Curb, gutter, paving, and demarcation of the right of way should be included as a requirement for all new development in the study area.
- Delineate how patrons should park within rights-of-way in the study area. Consistently sign the entire right-of-way, and enforce restrictions. Ensure that there are not multiple types of parking within one block face (for example, a single block face should require parallel parking only, not parallel parking and angled parking) to maximize potential inventory.

- Prior to winter months each year, publish a notice in the community newsletter with the slogan “Winter’s Coming! Are **You** Ready?” or similar to warn public of snow removal procedures and appropriate use of the right-of-way.
- Exempt residents from enforcement of Chapter 11.10 as necessary. Consider a Residential Parking Permit Program (RPP) in areas where extensive commercial usage of on-street parking (such as the on-street parking around A.G. Roofing) is significantly encroaching on residents’ ability to use the on-street supply. To execute the RPP, require an application with proof of address, as well as a nominal fee to cover administrative costs. Limit number of vehicles per household, or scale up fees to deter additional vehicles (e.g. free for first vehicle, \$20 for second vehicle, \$50 for third vehicle, etc.) Enforcement of the RPP could be limited to particular overnight hours to reduce any resulting increases to labor costs (e.g. 10 PM to 12 AM, or 12 AM to 2 AM), perhaps beginning on a daily basis at the onset and reducing to biweekly or weekly enforcement as the program matures. As noted previously, recommendations related to enforcement are further discussed in the “Time Restrictions and Enforcement” section below.



### *SIGNAGE AND WAYFINDING*

The Town has already made a number of efforts to improve signage in its downtown in recent years—a major beautification effort along Broadway and some wayfinding signage throughout the Town showcase Eagle’s main shopping and entertainment areas. However, at present, there is minimal signage and other means of information to guide parkers, and particularly first-time visitors, to appropriate inventory. This can result in some frustration among visitors, and can even lead to spillover into neighborhood/residential parking supply or private, restricted lots.

As discussed in Section 1, survey respondents expressed moderate support for improvements to parking- and mobility-related signage and wayfinding in Eagle’s downtown. However, as many respondents also indicated a lack of awareness of existing restrictions, parking locations, and related information, WALKER considers this a very important intervention.

WALKER recommends the following:

- Create an online “one stop shop” for parking information—where spaces are time limited, where visitors can park, and where there are other restrictions (e.g. resident parking only, etc.) Also include months out of the year where snow removal is done on a consistent basis, the process for snow removal and impacts on parking, etc. Include all information in a downloadable and printable PDF document.

Figure 3.3: Downtown Parking Map—Golden, CO

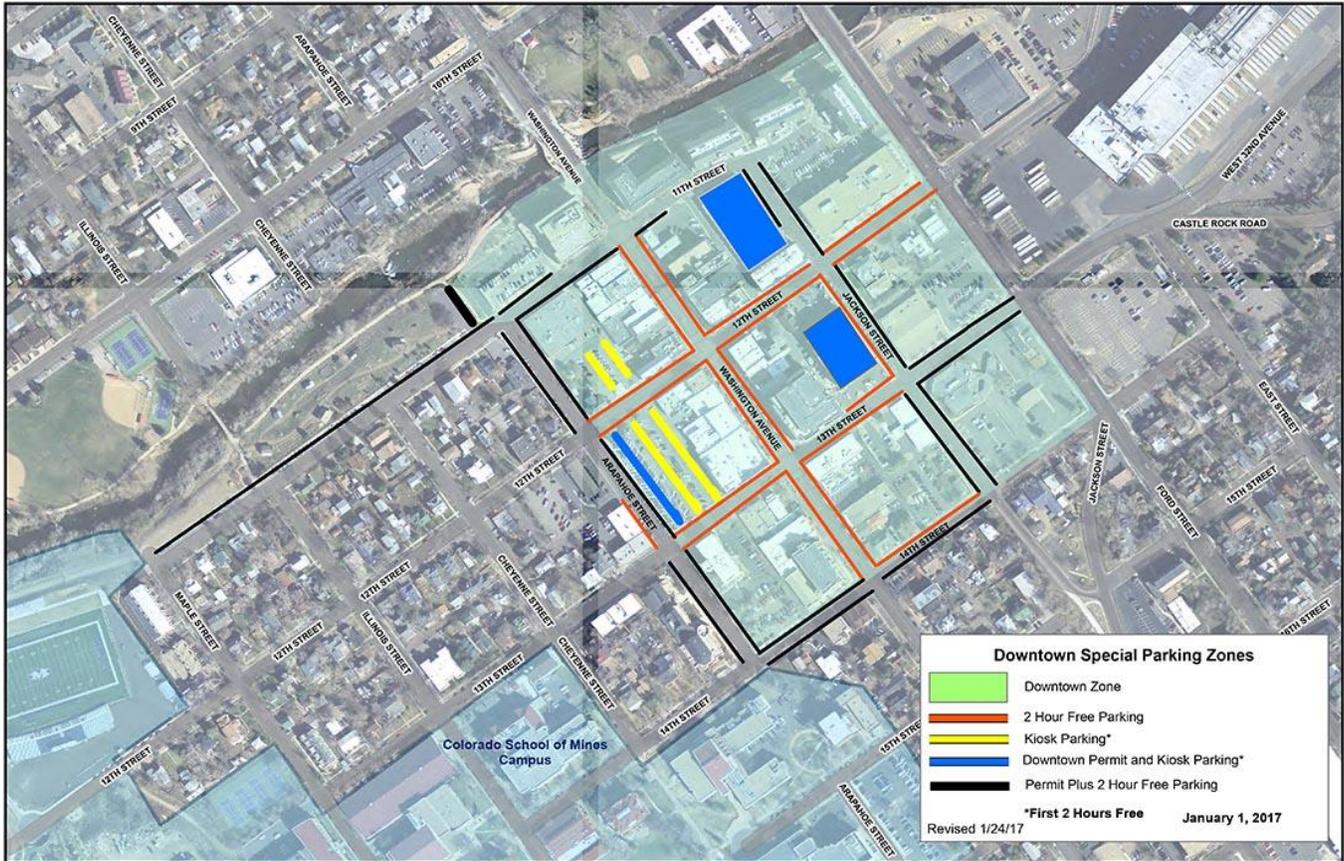
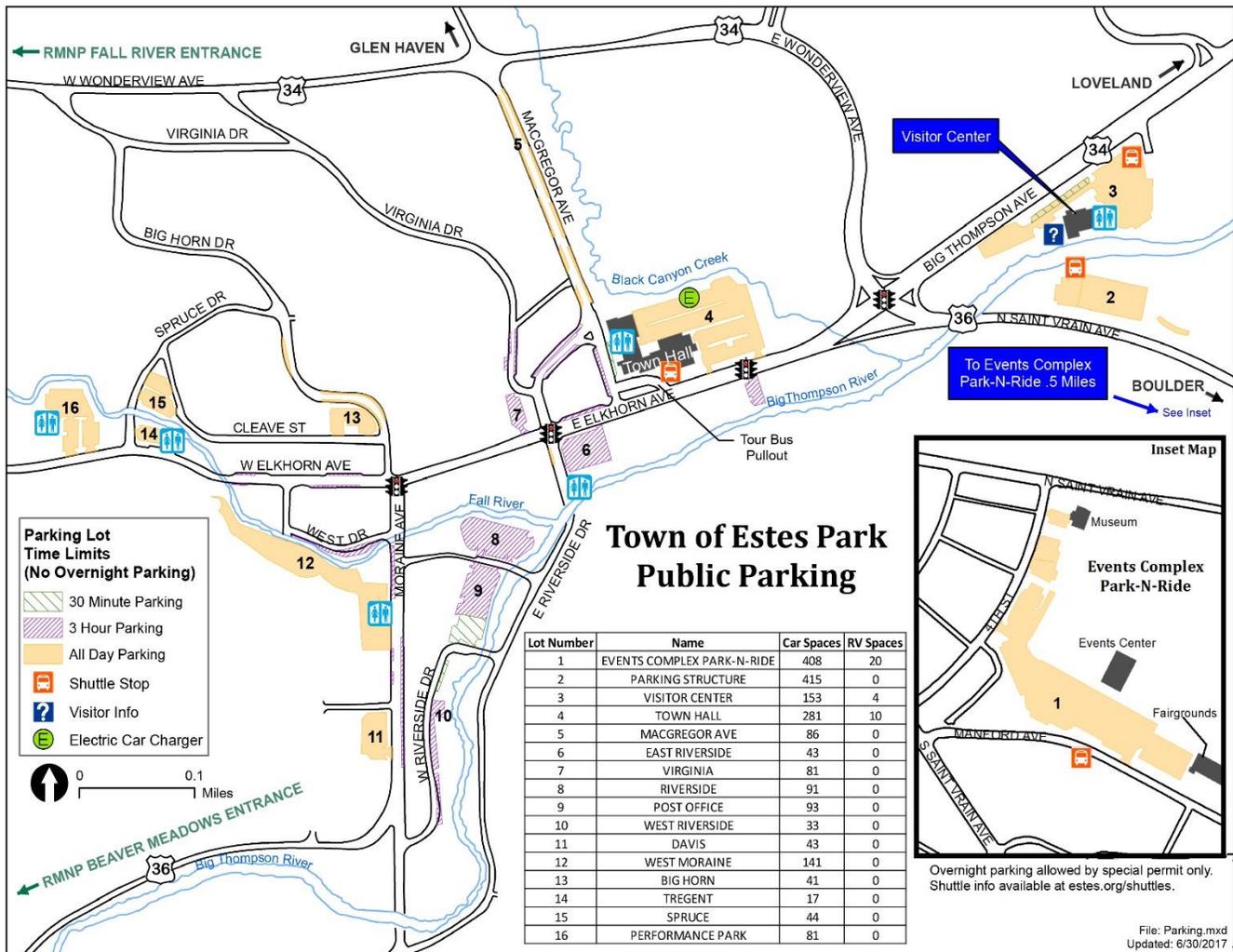


Figure 3.4: Public Parking Map—Estes Park, CO



- Leverage the Broadway signage as a color and style template, and develop consistent signage showing visitors to appropriate parking locations close to businesses and other attractions (e.g. on-street along Broadway, 2<sup>nd</sup> or 3<sup>rd</sup> streets).
- Ensure all signs showing restrictions (e.g. time limits along Broadway) are sized and angled in such a way that they can be read from a moving vehicle. Also ensure that signage is consistently placed, and shows where the restriction applies using arrows or some other means.

### *TIME RESTRICTIONS AND ENFORCEMENT*

The Town of Eagle has recently implemented two-hour time-limited parking along Broadway between Grand Avenue and 5<sup>th</sup> Street to encourage turnover of on-street spaces nearby popular shops, restaurants, and other attractions. WALKER encourages the Town to maintain this time-limited parking, and recommends the following additional steps:

- If not done already, issue a public notice in the community newsletter regarding the time-limited parking and its benefits. The major public-facing benefit of time limits is ensuring parking spaces regularly become available for customers and visitors, resulting in a more vibrant downtown. Most survey respondents expressed low support for time limits and strict enforcement, and had a limited understanding of existing restrictions; as such, it will be essential for the Town to embark on an extensive public outreach effort focused on this topic. This is further discussed in Section 5- Implementation.
- Include discussion of time limits and other parking restrictions in a public meeting, as discussed in Section 5 of this Plan.
- Hire (or otherwise allocate) one additional staff person dedicated to parking restriction enforcement.
- Maintain and enforce time-limited parking along Broadway; consider expansion to 2<sup>nd</sup> and 3<sup>rd</sup> Streets between Grand Avenue and Capitol Street as development intensifies and parking demand in those areas increase.
- Ensure some unrestricted parking on-street is maintained in peripheral areas for employees. An alternative that may be considered as Eagle's downtowns continue to develop is an employee parking permit program on peripheral streets—an option that would allow the Town to expand time limits to these peripheral streets while still allowing employees with limited alternative options to circumvent those time limits and park in one spot during their entire shift. WALKER does not recommend that the Town consider an employee parking permit program in the near future.



Figure 3.5: Commuter Neighborhood Parking Permit Application—Boulder, CO

**NPP: A SOLUTION FOR BOULDER'S NEIGHBORHOODS**

**SHARING THE STREETS:**

In 1996, the City of Boulder adopted a new Transportation Master Plan (TMP) with the support of its citizens. One item addressed was the need to manage parking, and balance transportation demands, while preserving the quality of life in Boulder.



In this effort the City developed a number of options, among them the use of residential permit parking system. Residential parking permit programs have been used successfully in cities nationwide and are most often implemented in neighborhoods adjacent to major employment and activity centers.

**PRESERVING THE CHARACTER OF OUR NEIGHBORHOODS:**

The Neighborhood Permit Parking Program (NPP) is designed to make Boulder neighborhoods safe and pleasant places to live, work and attend school by encouraging less driving and reducing on-street parking congestion. Each neighborhood in the program has public parking limits that are unique to that area and take into account the neighborhood's particular needs.

**NPP ZONES & PUBLIC PARKING TIME LIMITS:**

Columbine	9am - 5pm Mon - Fri	2 hour limit
East Aurora	8am - 6pm Mon - Fri	3 hour limit
East Ridge	9am - 5pm Mon - Fri	2 hour limit
Fairview	8am - 4pm School Days	2 hour limit
Goss Grove	8am - 6pm Mon - Fri	2 hour limit
High / Sunset	8am - 6pm Mon - Fri	2 hour limit
Mapleton	8am - 6pm Mon - Fri	3 hour limit
University Heights	8am - 8pm Mon - Sat	2 hour limit
University Hill	9am - 5pm Mon - Fri	2 hour limit
West Pearl	8am - 6pm Mon - Fri	3 hour limit
Whittier	8am - 8pm Mon - Fri	3 hour limit
Whittier (Night)	8pm - 12am Fri & Sat	No Parking Without permit

**Commuter NEIGHBORHOOD NPP Permit Application**



**INFORMATION & APPLICATION**

**Community Vitality**  
Parking & Access | Economic Vitality | District Management  
1500 Pearl Street, Suite 302 • Boulder, CO 80302  
Ph: 303.413.7300 • Fax: 303.413.7301 boulderparking.com

- Prior to any increase in enforcement, issue a public notice reminding community members of the restrictions (include a map of where restrictions are located and information about when and how they are enforced).
- Train any staff conducting enforcement as “ambassadors”, knowledgeable about parking restrictions, where to park, etc.
- Issue courtesy warnings for the first 90 days after enforcement is ramped up.
- As Eagle’s downtown densifies and parking demand increases, consider “zone-based” enforcement with handheld License Plate Recognition (LPR) units. Should this option be pursued, WALKER anticipates an initial all-in capital investment in the \$8,000 to \$10,000 range.
  - **What is “zone-based” enforcement?** The Town could consider enforcing by zone (such as the Broadway Corridor), rather than by space, in time limited areas. This would bolster prevention of the “employee shuffle” and other issues common to downtowns, as users would have to move out of the “zone” rather than simply to another space within the zone. With a digital enforcement system (discussed below) zones could be geo-fenced, and license plates present in the same zone for more than two hours would warrant a warning or ticket. This would be much more challenging to enforce with a manual system and WALKER does not suggest that staff attempt to accomplish this; however, language describing this policy and the corresponding zones could be adopted in Town ordinance and included on time-limited signage throughout the downtown to discourage violations.

- **What is a handheld LPR unit?** Handheld LPR units can read and record license plates and process data to identify violators. Though less efficient than a vehicle-mounted LPR unit, handheld LPR units are more cost-friendly and can be integrated into existing manual enforcement routes while increasing efficiency.



### ADA AUDIT

During conversations with Town Staff, some concern was expressed regarding the sufficiency of ADA parking in Eagle's downtown. WALKER recommends that the Town consider an ADA Audit to ensure that provided ADA parking is meeting the regulations set forth in the Americans with Disabilities Act Guidelines (ADAG). This audit should include not only an inventory of ADA supply as compared to total provided inventory, but also evaluation of other factors pursuant to ADAG, such as slope, path of travel, signage, etc.



04 Code and  
Policy

## SECTION 4: CODE AND POLICY

The purpose of this section is to discuss code and policy recommendations for parking in Eagle’s downtown, based on a review of existing regulations in the Town’s municipal code as compared to industry best practices and Eagle’s future development goals. It should be noted that a significant number of survey respondents cited the stringent off-street parking requirements as a deterrent to development in Eagle; additionally, Town Staff noted the high number of variances granted in the downtown waiving off-street parking requirements, thereby undermining existing regulations and taking up a significant amount of staff and elected official resources.

**How do code requirements impact parking supply?** Off-street parking requirements set forth in a community’s municipal code do more than merely impact the sheer number of parking stalls associated with individual developments. They also affect other aspects of parking supply, including:

- **Usability of parking supply:** Private developers can gate, sign, tow, or otherwise manage their own off-street parking. This means that a community or downtown can have a lot of parking that can’t be utilized by most parkers, leading to inefficiencies in the system.
- **Demand placed on on-street or other public parking supply:** Restrictions placed on private parking supply, or limitations on private parking supply, can increase demand on public parking resources, such as on-street parking.

**How can code requirements further parking and mobility policy?** Off-street parking requirements aren’t simply arbitrary requirements for off-street parking provision by land use—they can also play a significant role in forwarding a community’s parking and mobility goals. While parking and mobility policy will ultimately be under the guise of key decision makers—namely Eagle’s Town Board—WALKER has provided a number of code and policy changes based on industry best practices and initial feedback from community members.

### HOW DO OTHER COMMUNITIES EAGLE’S SIZE DEAL WITH CODE REGULATIONS?

Municipalities, particularly smaller municipalities, have been dealing with off-street parking requirements in the same manner for quite a long time, despite the changing landscape of transportation and mobility as a whole. The overwhelming majority of cities and towns use traditional off-street parking requirements, meaning that a certain number of off-street stalls is required for certain land uses. Developments that include multiple land uses are generally required to provide the sum of off-street parking stalls required for each individual land use. While this methodology generally ensures sufficient parking for all land uses, it frequently results in a number of negative consequences, including excess supply, lack of efficiency realized for mixed-use developments that can share parking resources among multiple land uses (e.g. a mixed-use building with residential and office components), and encouragement of single-occupancy vehicle infrastructure over alternative and multimodal infrastructure. It can also, as in Eagle’s case, result in frequent variance applications attempting the waive the same regulation, thereby undermining the regulation and taking up valuable staff and elected official resources.

Often, more progressive off-street parking strategies are used by larger cities and towns; Denver, for example, offers a variety of reduction opportunities from traditional off-street parking requirements, including:

- Shared parking reduction for mixed-use development.

- Reductions for multimodal infrastructure, such as bike and car shares (up to 25% reduction allowable administratively, or up to 50% reduction with Commission approval).
- Flat ratio for all land uses in certain districts (e.g. 1 space per 750 sq. ft. of all new commercial development in the Lower Downtown district).

Some of Eagle’s peer communities—notably Salida and Buena Vista—have updated their off-street parking regulations in recent years to allow for added flexibility in how the private market provides parking.

- **Salida (population 5,581):** Developers can pursue a number of alternative options instead of following traditional off-street parking requirements, including:
  - Leasing existing parking off-site to fulfill parking requirements instead of constructing new parking;
  - With review and approval from the Planning Board, focusing on arrivals via alternative modes (e.g. public transportation) instead of vehicular modes, thereby allowing for a reduction in parking spaces provided on-site;
  - With review and approval from the Planning Board, sharing parking among two or more complementary uses, typically allowing for a reduction in parking spaces provided on-site.
- **Buena Vista (population 2,778):** Non-residential development in the downtown core can provide a per-space financial contribution to the Town in lieu of providing required off-street parking.

#### HOW CAN EAGLE BETTER ALIGN WITH OFF-STREET PARKING BEST PRACTICES?

At present, the Town of Eagle primarily utilizes traditional off-street parking requirements (a prescribed number of stalls required based on land use) to regulate how the private market provides off-street parking. However, the Town has adopted a number of reduction opportunities, including:

- **“Grandfathered” parking requirements for conversions in the Broadway and Central Business Zone Districts:** Section 4.07.050 A.3 of Eagle’s municipal code allows for developers to change and/or intensify existing building uses in Eagle’s downtown core without adding new parking.
- **Shared parking:** Section 4.070.050 H of Eagle’s municipal code allows for developers of mixed-use buildings to petition for a reduction in off-street parking requirements through shared parking, wherein multiple complementary uses would share parking resources. This section requires a process similar to a variance process (except for petitioners don’t have to prove hardship), and includes review and approval by the Planning Commission.

While these measures certainly offer some relief to developers, WALKER recommends several action steps to better align Eagle’s off-street parking requirements with industry best practices, in service of the following goals:

- Alleviation of some of the parking-related financial and practical deterrents for infill development in Eagle’s downtown.
- Reduction in the number of off-street parking related variances granted to developers.

- “Right-sizing” of parking supply based on existing demand patterns, meaning that required off-street parking supply will match actual demand.
- Encouragement of multimodal infrastructure and opportunities.

The following action steps are recommended:

- Consider adopting a uniform off-street parking requirement of 1.5 or 2 spaces per 1,000 square feet of new development in the Central Business Zone and Broadway Districts.
- Maintain existing off-street parking requirements outside of these Districts, with a review every two years by the Planning Commission to determine whether and where to extend the uniform off-street parking requirement of 1.5 or 2 spaces per 1,000 square feet.
- Include a separate off-street parking requirement for studio and “micro” multifamily residential units at 1 space per unit.
- Amend Section 4.070.050 H to require a shared parking study performed by a third party experienced in producing such studies. Allow for up to a 25% reduction from the required off-street parking with approval from the Community Development Director; reductions higher than 25% should require approval from the Planning Commission.
- Allow for a reduction of up to 10% with approval from the Community Development Director for multimodal infrastructure, such as bike share, fixed bike parking, fixed and covered bike parking, car share, etc.
- After new parking requirements and reduction opportunities are in effect, review development impact annually for five years. This review should be performed by the Community Development Director, with report-outs to the Planning Commission and Town Board.

For decision-makers’ reference, the following figure (Figure 4.1) provides a general overview of different types of off-street parking requirement frameworks, and the pros and cons of each.

Figure 4.1: Pros and Cons of Off-Street Parking Requirement Frameworks

Policy Approach	Description	Pros	Cons
Traditional off-street parking requirements	Per unit of land use; in Eagle, varied requirements but generally 1-.5 to 2 stalls/unit for residential, and up to 10 to 15 per 1,000 sq. ft. for most commercial and service uses	Ensures that most uses are providing parking for most or all of the parking demand they generate; limited impact to on-street and other public resources; lenders are most comfortable with this model	Generally results in over-supply of parking, especially in a mixed-use environment like downtown Eagle; can also lead to lower densities and development sprawl
Traditional off-street parking requirements, with allowable reductions	Same as above, but with percentage-based reductions or exemptions for things like shared parking, affordable or workforce housing, or TDM interventions	Ensures that most uses are meeting most or all of the parking demand they generate, but also encourages achievement of various policy goals, like urban infill, multimodal options, etc.	Benefits some but not all projects; promotes lower density overall with pockets of higher density where projects have fulfilled the reduction/exemption requirements
Blended and reduced requirements	Requires a blended ratio for all developments, regardless of use, in line with average demand for a dense environment supported by multimodal options	Allows for increased development density, especially when paired with other reductions such as shared-use reductions, or policies like Fee In-Lieu.	Public parking system generally relied on to address overflow demand; often requires a paid public parking system to pay for parking provision and operations/maintenance costs associated with providing parking for many users
Zero requirements	All parking requirements are waived	Allows for significant development density.	Public parking system generally relied on to address most or all demand; often requires a paid public parking system to pay for parking provision and operations/maintenance costs associated with providing parking for most users



# 05 Implementation

## SECTION 5: IMPLEMENTATION

Implementation is, of course, an essential step for any plan—without a well-executed implementation process, plans will merely sit on a shelf collecting dust. For the Town of Eagle, WALKER envisions an implementation process focused on two key components: community outreach and support, and funding opportunities.

**Even the best plans don't succeed without community support.** Public education and overall buy-in is essential—particularly for parking changes that will impact how people move around their community. Developing and implementing plans without this component often results in confusion—even anger—about the changes. As Eagle moves towards a more formalized parking system, it is important to educate and engage the public not only on the changes being considered, but also *why* they are being considered, and what benefits they offer to the community.

**Even the best plans don't succeed without funding.** A successful parking system that serves its users well doesn't only need money for physical parking spaces—decision-makers must also budget for staffing, signage and wayfinding, and administrative efforts. WALKER recommends that, in the near-term, Eagle focus on using existing budgeting mechanisms—such as allocation of General Fund monies, the creation of a separate enterprise fund for parking and mobility, or allocation through capital improvement plans—to generate capital for the low- to mid-cost interventions discussed in this report. WALKER has also included an appendix detailing a number of alternative funding mechanisms for the Town's consideration, each of which will create a pipeline of funds for future improvements and changes to the parking system.

## COMMUNITY OUTREACH

The scope of work for this Plan included a very limited public participation process via a digital survey generating strong participation, with 368 respondents. Based on the community's response to the survey and industry best practices, WALKER recommends a holistic public education and participation process to guide and refine parking system changes. This process could include the following initiatives:

- **Public Outreach Meetings.** Many Eagle residents seemed unaware of current parking restrictions based on survey responses. As such, prior to adopting any particular changes or recommendations, WALKER recommends the following public outreach meetings:
  - Meeting #1: Education focused, covering the following topics:
    - What are the existing parking restrictions in Eagle, and what are their purposes?
    - Why is the Public Right-of-Way an important resource for our community?
    - What is formalized on-street parking and what are its benefits?
  - Meeting #2: Visioning focused, covering the following topics:
    - What aspects of parking in downtown Eagle work well?
    - What aspects of parking in downtown Eagle need improvement?

- What does Eagle’s downtown parking system need more of (employee parking, visitor parking, multimodal options, etc.)?
- As Eagle changes over time, what should parking look like?
- **Parking Advisory Committee.** To help bridge the gap between “Town interests” and “stakeholders,” this Plan recommends that the mayor and/or Board appoint a Parking Advisory Committee comprised of representatives from several different constituencies. Parking Advisory Committees can offer ongoing stakeholder input and involvement for changes to the parking system and advise on parking and transportation related issues. Communities like Estes Park and Fort Collins in Colorado have formed these committees, as well as hundreds of cities and towns nationwide.
  - **Website.** Develop and regularly maintain a webpage or website with downtown parking information, restrictions, and frequently asked questions.

## FUNDING

WALKER recommends that Eagle take near-term (1 to 2 years) action to fund the following recommendations discussed in this report:

- One additional staff person to assist in regular enforcement of existing parking restrictions and other restrictions as added (e.g. demarcation of how to park within a certain block face).
- Signage to demarcate how patrons should park within rights-of-way in the study area and show drivers where parking is available.
- Staffing and/or consultant time to create parking brochure and other communications with Eagle citizens.

In the medium term (5 to 7 years), WALKER recommends that Eagle take action to fund the following additional items:

- Paving and striping of unpaved rights-of-way dedicated to on-street parking.
- Handheld LPR units for enforcement staff.

WALKER has identified several funding options, including case studies of each, to assist Eagle in raising capital for near-term, mid-term, and long-term parking system updates beyond those outlined—for example, to add additional inventory, pave and stripe extensive portions of the downtown on-street parking supply, hiring additional staff, etc. These funding mechanisms will also ensure the Town has ample means for paying for system costs without implementing a paid parking system for end users.

Case studies are derived from communities of considerably larger size than the Town of Eagle—this is because very few towns of Eagle’s size leverage these techniques, and more often use portions of their General Fund or Capital Improvement Funds to finance parking system upgrades. However, WALKER has outlined some alternative financing mechanisms, given Eagle’s proactive and progressive leadership style, and opportunities in the community for continued growth. These mechanisms are defined and discussed in the appendix.



# 06 Appendices



STUDY AREA  
BOUNDARY



SITE PLAN



NORTH

Legend

- ⊗ Block Identification

NORTH





## ON STREET PARKING MAP

**Legend**

- ⊗ Block Identification
- Time-Limited Parking
- Unrestricted Formal Parking
- Unrestricted Informal Parking

NORTH





1 PM TOTAL  
OCCUPANCY  
(PEAK TIME)



SITE PLAN

Legend

Block Identification

Occupancy

- 0% - 59%
- 60% - 74%
- 75% - 89%
- 90% - 100%





1 PM TOTAL  
OCCUPANCY  
(PUBLIC)



SITE PLAN

Legend

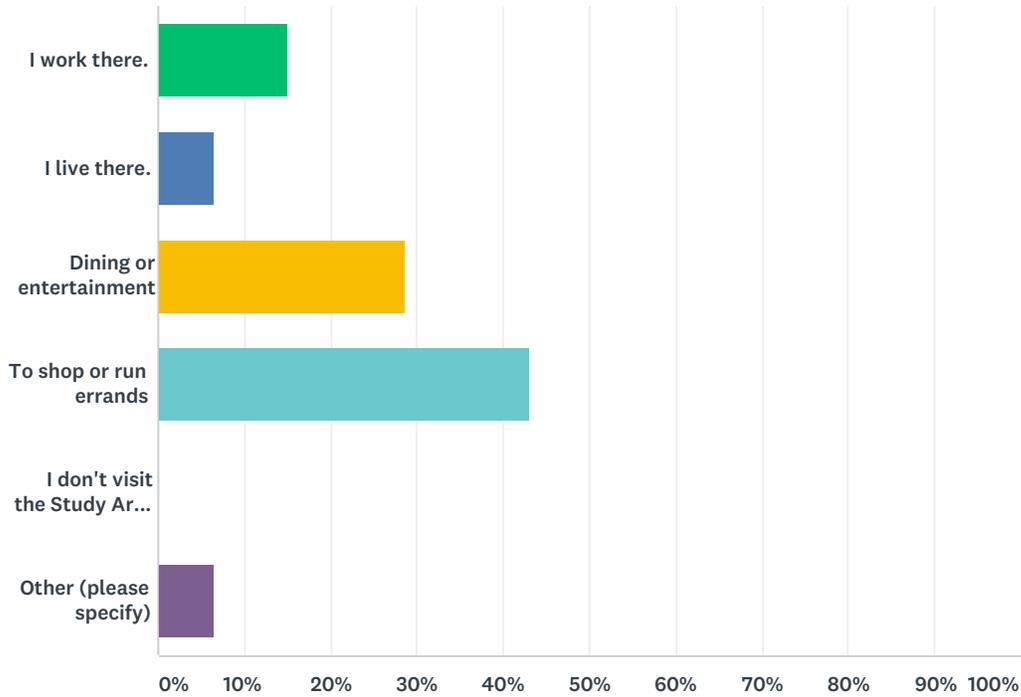
Block Identification

Occupancy

- 0% - 59%
- 60% - 74%
- 75% - 89%
- 90% - 100%

# Q1 What is your most common reason for visiting the 13- block Study Area? (see map below)

Answered: 367 Skipped: 1



ANSWER CHOICES	RESPONSES	
I work there.	14.99%	55
I live there.	6.54%	24
Dining or entertainment	28.61%	105
To shop or run errands	43.05%	158
I don't visit the Study Area.	0.27%	1
Other (please specify)	6.54%	24
<b>TOTAL</b>		<b>367</b>

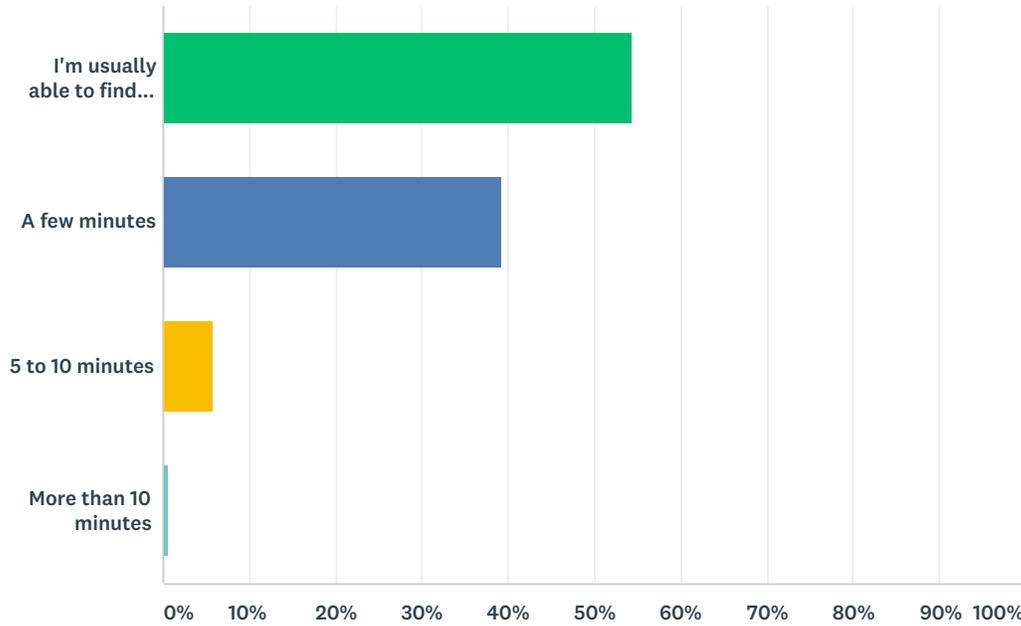
#	OTHER (PLEASE SPECIFY)	DATE
1	Live and work here!	10/16/2017 2:41 PM
2	both I work there and dining/entertainment	10/15/2017 8:40 AM
3	Family member and I live there n separate locations.	10/14/2017 9:20 PM
4	I visit there regularly for all reasons except living in the study area	10/14/2017 10:11 AM
5	Shop and Dentist	10/14/2017 6:43 AM
6	weekly business meeting	10/14/2017 6:37 AM
7	Dentist	10/13/2017 8:59 PM
8	Appointments, errands and the park	10/13/2017 2:05 PM

## Town of Eagle Downtown Parking Survey

9	Pharmacy, coffee, hair salon	10/13/2017 1:31 PM
10	Town offices	10/13/2017 1:15 PM
11	Hair appointment and massage appointments	10/13/2017 12:26 PM
12	Banking and haircut, sometimes coffee	10/13/2017 11:25 AM
13	Coffee and hair appts plus shopping	10/13/2017 11:12 AM
14	Church	10/13/2017 10:58 AM
15	I both live and work there, and shop at Eagle Pharmacy, eat at Red Canyon Cafe and get burgers to go from Brush Creek Saloon.	10/12/2017 6:45 PM
16	Professional services, retail shopping	10/12/2017 5:03 PM
17	Visit Town Hall	10/12/2017 4:37 PM
18	Drop off for World Academy @ Red Canyon	10/12/2017 4:17 PM
19	I live at 533 Washingto which is one block out of the study area. Parking is a real hassle during any event. My husband frequently refused to leave home as he's afraid he won't have a place to park when we return. We feel trapped	10/12/2017 3:45 PM
20	Some shopping, but mostly to get my hair cut & colored	10/12/2017 2:40 PM
21	I work at the county and usually park on 5th Street	10/12/2017 1:34 PM
22	calling on clients in that area	10/12/2017 12:18 PM
23	Bike shop, Yeti's, or Bonfire	10/12/2017 12:13 PM
24	Also work	10/12/2017 11:50 AM

## Q2 On average, how much time do you spend looking for a parking space once you arrive in the study area?

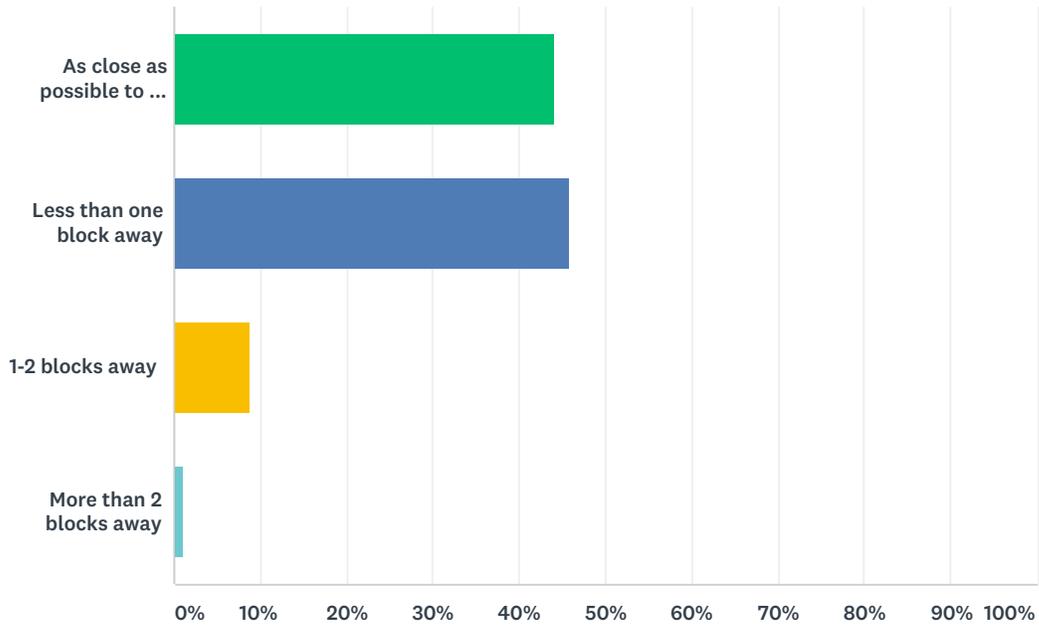
Answered: 364 Skipped: 4



ANSWER CHOICES	RESPONSES	
I'm usually able to find parking immediately.	54.40%	198
A few minutes	39.29%	143
5 to 10 minutes	5.77%	21
More than 10 minutes	0.55%	2
<b>TOTAL</b>		<b>364</b>

### Q3 How far from your destination do you typically prefer to park?

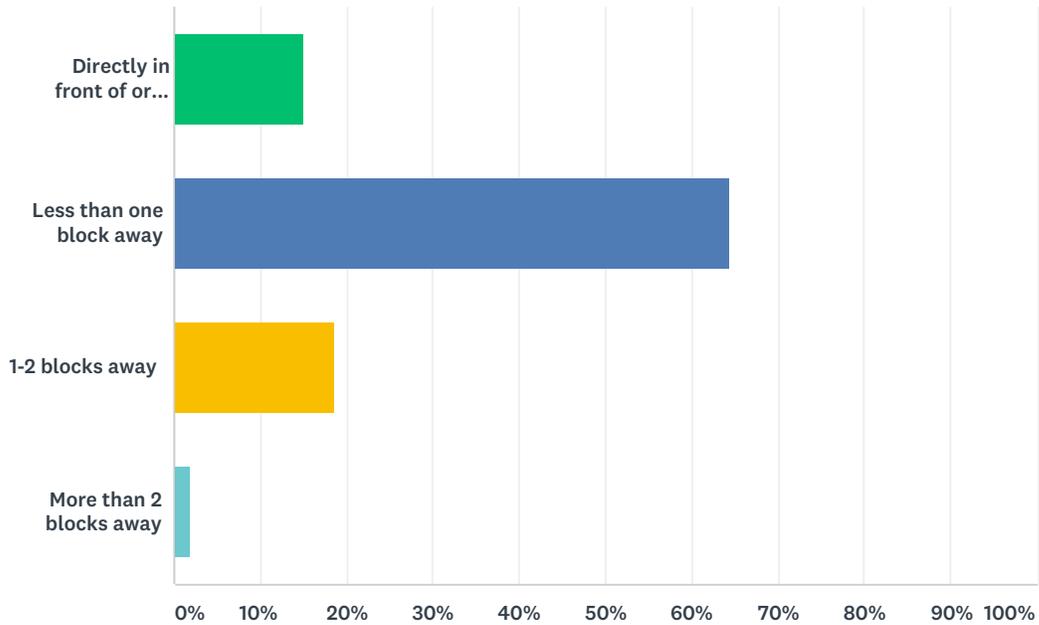
Answered: 360 Skipped: 8



ANSWER CHOICES	RESPONSES	
As close as possible to my destination	44.17%	159
Less than one block away	45.83%	165
1-2 blocks away	8.89%	32
More than 2 blocks away	1.11%	4
TOTAL		360

## Q4 How far from your destination are you typically able to park?

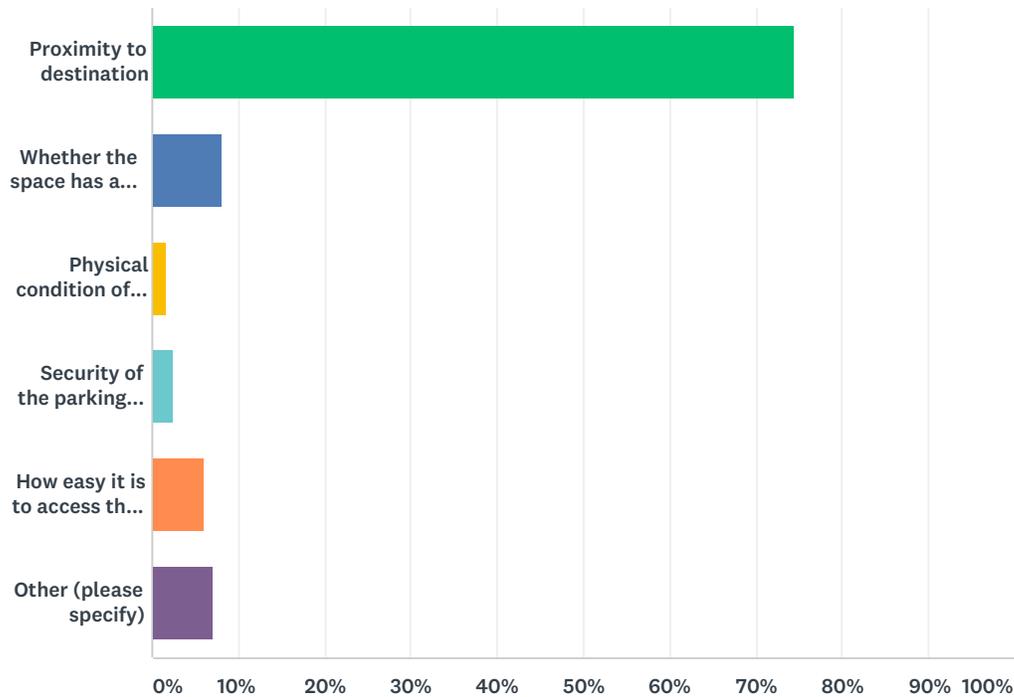
Answered: 365 Skipped: 3



ANSWER CHOICES	RESPONSES	
Directly in front of or adjacent to my destination	15.07%	55
Less than one block away	64.38%	235
1-2 blocks away	18.63%	68
More than 2 blocks away	1.92%	7
TOTAL		365

## Q5 Which factor is most important to you when deciding where to park on a typical day?

Answered: 364 Skipped: 4



ANSWER CHOICES	RESPONSES	
Proximity to destination	74.45%	271
Whether the space has a time limit	8.24%	30
Physical condition of the parking area	1.65%	6
Security of the parking area	2.47%	9
How easy it is to access the parking area from main roads	6.04%	22
Other (please specify)	7.14%	26
<b>TOTAL</b>		<b>364</b>

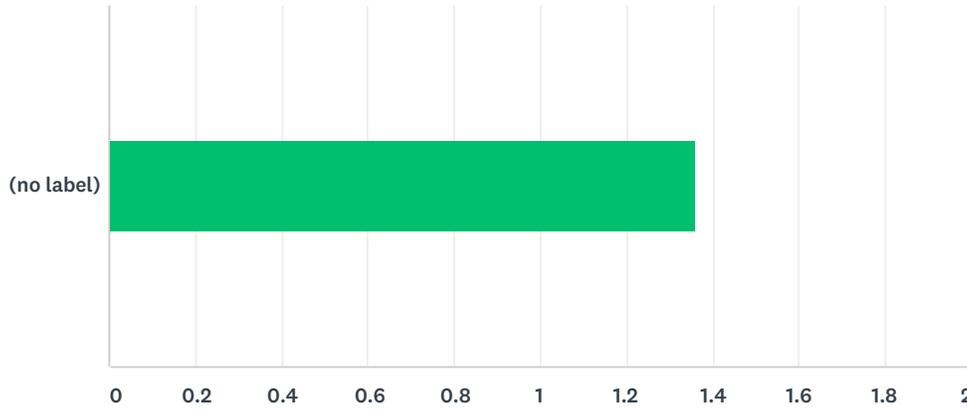
#	OTHER (PLEASE SPECIFY)	DATE
1	in the shorter days I like to be where there is plenty of light to walk to my car and feel safe	10/20/2017 9:35 AM
2	Close to my job	10/18/2017 4:57 PM
3	Do not want my car dinged or bumped	10/18/2017 8:06 AM
4	I actually don't drive down town. I always ride my bike. I'd prefer more secure bike racks than parking spaces. :)	10/17/2017 9:22 AM
5	I do not shop or spend money at any of the Downtown shops or restaurants, so parking in that area is not an issue I am concerned with.	10/15/2017 1:35 PM
6	weather	10/15/2017 8:40 AM
7	I	10/15/2017 6:57 AM

## Town of Eagle Downtown Parking Survey

8	ability to load/unload large objects out of car or putting children in carseats.	10/14/2017 4:41 PM
9	Size of space	10/13/2017 7:21 PM
10	Being able to park my bicycle in a safe area.	10/13/2017 6:01 PM
11	Least chance of door dings.	10/13/2017 5:15 PM
12	Easiest	10/13/2017 10:09 AM
13	Given that security and condtions are already well addressed, and we don't have limitations, I mostly consider proximity	10/13/2017 10:06 AM
14	I don't have an issue parking. I'm far more concerned about where my clients will park.	10/13/2017 9:02 AM
15	n/a live there and have driveway to park in	10/13/2017 8:15 AM
16	That parking doesn't dominate the whole area	10/13/2017 7:33 AM
17	Ease of enteringing and exiting spot in terms of traffic	10/12/2017 5:46 PM
18	How easy it is to park and how busy it is in the area. I will go other places and not stop if parking is too congested...	10/12/2017 4:37 PM
19	Easy ingress and egress into space	10/12/2017 4:17 PM
20	As stated before, I would like event parking to be prohibited in front of residential properties. At 60 plus is is difficult to schlep groceries any distance. also my 83 year old mother often visits on weekends	10/12/2017 3:45 PM
21	truly all- I guess security would be the ultimate decider but all	10/12/2017 2:52 PM
22	Need for handicapped parking.	10/12/2017 1:01 PM
23	Summer shade/Winter sun/not near other vehicles	10/12/2017 12:22 PM
24	It's most important to do my errands regardless of where I park	10/12/2017 12:18 PM
25	We have private parking at 129 Broadway	10/12/2017 12:04 PM
26	Staying far from idiots that give me door dings.	10/12/2017 12:00 PM

**Q6 Please share your level of support for the following parking management practices. Clear delineation of where and how to park on-street (striping and signage indicating any restrictions)**

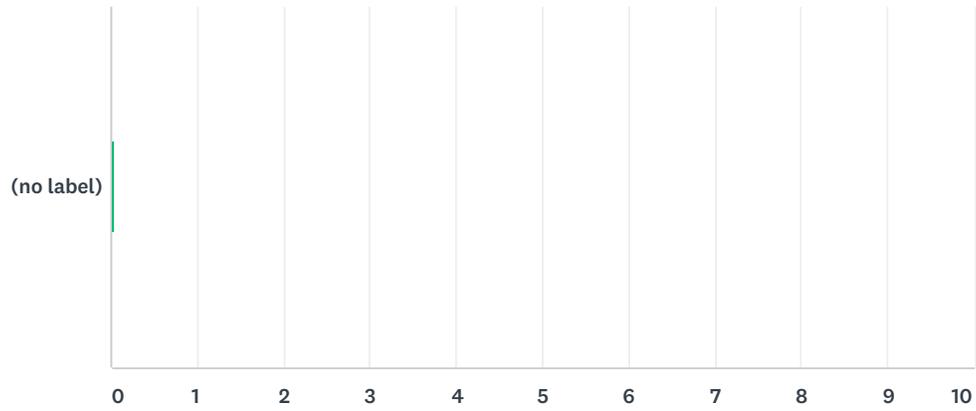
Answered: 343 Skipped: 25



	NO OPINION	DON'T SUPPORT	MODERATE SUPPORT	STRONG SUPPORT	TOTAL	WEIGHTED AVERAGE
(no label)	4.66% 16	7.00% 24	33.24% 114	55.10% 189	343	1.36

## Q7 Time limits for on-street parking

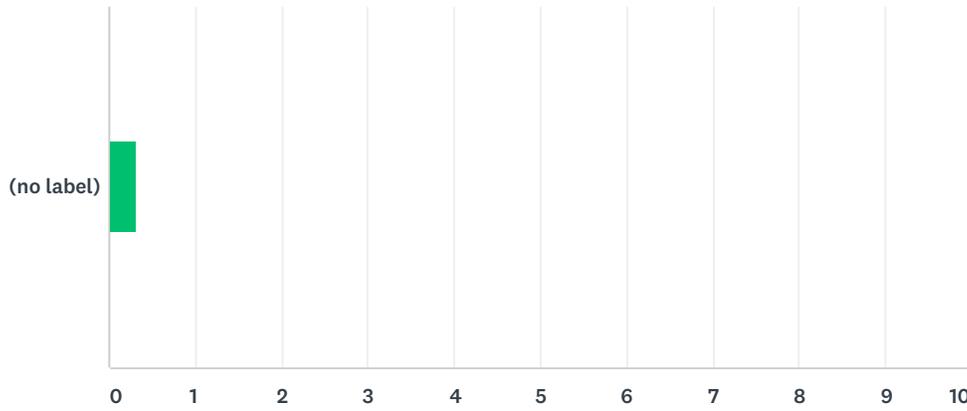
Answered: 345 Skipped: 23



	NO OPINION	DON'T SUPPORT	MODERATE SUPPORT	STRONG SUPPORT	TOTAL	WEIGHTED AVERAGE
(no label)	3.77% 13	51.30% 177	33.91% 117	11.01% 38	345	0.05

## Q8 Strict enforcement of parking regulations

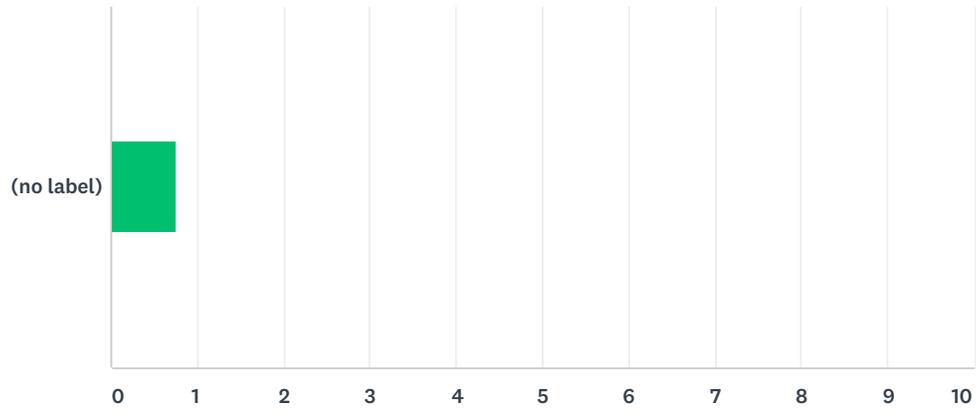
Answered: 345 Skipped: 23



	NO OPINION	DON'T SUPPORT	MODERATE SUPPORT	STRONG SUPPORT	TOTAL	WEIGHTED AVERAGE
(no label)	5.80% 20	37.39% 129	44.06% 152	12.75% 44	345	0.32

## Q9 Signage and wayfinding program to help locate parking

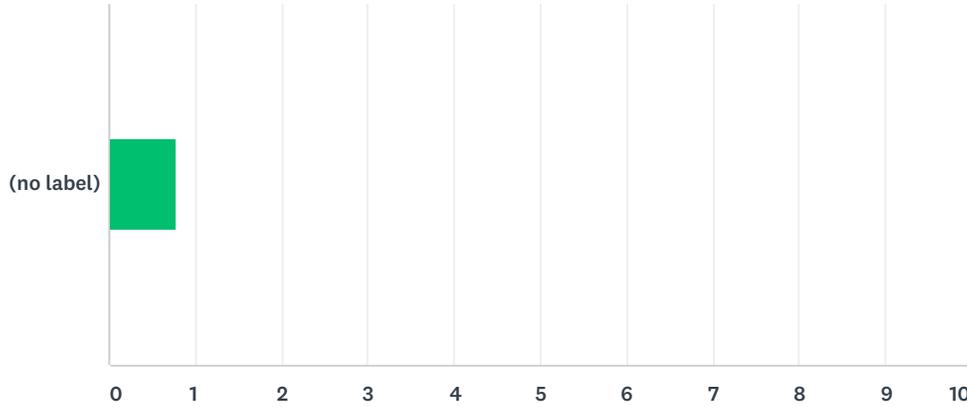
Answered: 343 Skipped: 25



	NO OPINION	DON'T SUPPORT	MODERATE SUPPORT	STRONG SUPPORT	TOTAL	WEIGHTED AVERAGE
(no label)	11.66% 40	18.66% 64	46.06% 158	23.62% 81	343	0.75

## Q10 Designated parking for specialty vehicles (such as large trucks and trailers)

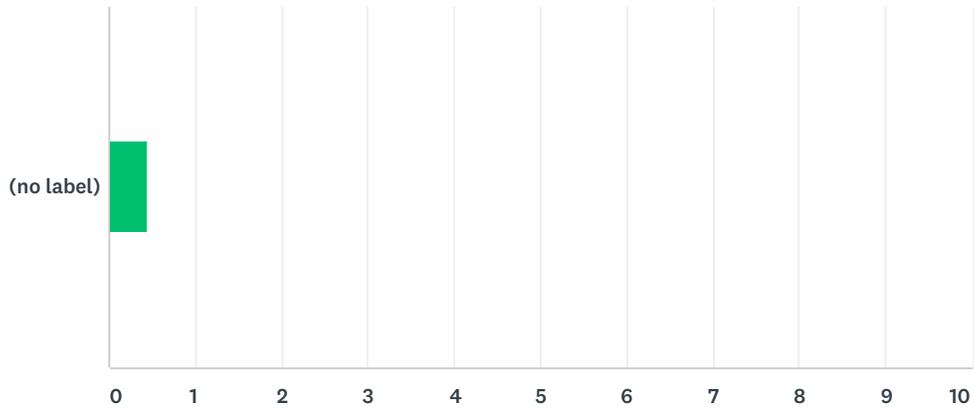
Answered: 344 Skipped: 24



	NO OPINION	DON'T SUPPORT	MODERATE SUPPORT	STRONG SUPPORT	TOTAL	WEIGHTED AVERAGE
(no label)	14.53%	17.44%	41.57%	26.45%	344	0.77
	50	60	143	91		

## Q11 Residential Parking Permit programs

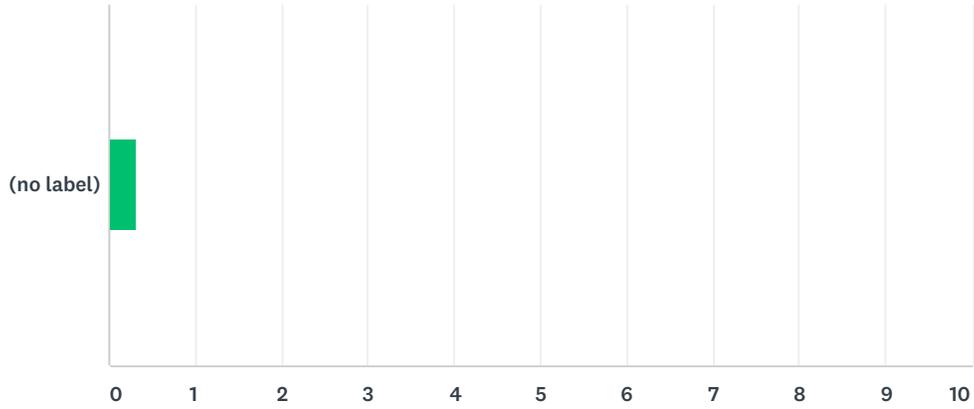
Answered: 340 Skipped: 28



	NO OPINION	DON'T SUPPORT	MODERATE SUPPORT	STRONG SUPPORT	TOTAL	WEIGHTED AVERAGE
(no label)	16.47% 56	45.29% 154	26.18% 89	12.06% 41	340	0.43

## Q12 Employee Parking Permit programs

Answered: 343 Skipped: 25



	NO OPINION	DON'T SUPPORT	MODERATE SUPPORT	STRONG SUPPORT	TOTAL	WEIGHTED AVERAGE
(no label)	17.49% 60	32.94% 113	34.99% 120	14.58% 50	343	0.31

Town of Eagle Downtown Parking Survey

Q13 Please provide any additional comments on your responses here (no character limit).

Answered: 142 Skipped: 226

#	RESPONSES	DATE
1	I have never experienced any parking related inconvenience in downtown Eagle so I don't really support limited duration parking and/or enforcement because I don't perceive a problem. I've never had to park more than a block from my destination. However, I do not live within the study area so my experience relates primarily to businesses on Broadway.	10/30/2017 10:07 PM
2	Parking is fine.. please use our money elsewhere and get rid of the parking enforcement. Tagging another person's property (marking tires) is vandalism.	10/27/2017 3:43 PM
3	Hi, It's Nancy Powell here.. I maintained a successful business in downtown Eagle for nearly 22 years. (Still own the building, however it is on the market with little serious interest due to I believe the lack of focus of the town! Thanks a lot! ) In any case, when there is a vital downtown there is a parking issue, otherwise not so much..I required all my employees to park off Broadway and made spots available on my property. My tenants have let me know that adjacent businesses often park right in front of the building, what is with that? Does not seem to be much cooperation/empathy/support with the downtown businesses. Perhaps a bit more cooperation/communication between downtown businesses??? TWO downtown "schools"...REALLY not conducive to sales tax revenue...perhaps EAGLE needs to figure out where you are headed. Concerts to support the Bonfire or a concerted effort to support ALL downtown businesses, Thank you for the soap box!!! Good Luck, Nancy Powell	10/24/2017 11:33 PM
4	Don't let parking take over downtown ie large parking lots instead of buildings. keep and focus on a walkable bikeable downtown.	10/24/2017 4:10 PM
5	The only time I have a hard time parking within a block of my destination is during a major event such as flight days or Christmas on Broadway. I often park directly in from of destination.	10/24/2017 3:14 PM
6	If ppl could just park correctly in one spot and not take up several then this could help. Shouldn't be a restrictions on time on the main street its all very limited.	10/20/2017 9:41 AM
7	Street parking is the only parking available to my home. Without town parking I would not have a place to park our vehicles. Any changes to parking rules could impact how I access my house.	10/18/2017 8:14 AM
8	We're a small town--don't overdue this! This is not downtown Denver. Parking spaces should have been sufficiently planned for the type of business. Be careful if considering a parking meter program because the downtown business community is very fragile.	10/17/2017 8:35 PM
9	I think the town should be thinking about solid ways to support alternative ways to spend time down town without having to drive a car. The bike paths for example, are a great way to get from Eagle Ranch to down town. Most of the time I spend time trying to find a secure bike rack close to my destination. I'd like to see expanded bike infrastructure (bike lanes, racks, maybe a bike share?) and maybe think about free parking for car pooling vs driving by yourself. I'd like to avoid paid parking, but at the same time I don't think adding more parking spots is a long term answer. It's like adding another lane to the highway; It only helps for a small window of time.	10/17/2017 9:29 AM
10	Given the aging population in our town, additional handicap spaces on every block would be very beneficial. Also, if the sidewalks from other parts of town to the downtown area were more attractive, perhaps more people would walk.	10/16/2017 4:42 PM
11	YOU DO NOT HAVE ENOUGH PARKING ON BROADWAY FOR THE AMOUNT OF BUSINESS THAT DONE. YOU ALREADY TOOK 45 PARKING SPOTS DURING THE BEAUTIFICATION PROCESS THAT A LOT OF MY CLIENTS PARK IN THE ALLEY BEHIND MY OFFICE BECAUSE THERE IS NOWHERE TO PARK. IN THE WINTER, THERE IS EVEN MORE OF A PROBLEM WITH THE ELDERLY, HANDICAPPED AND MEDICALLY NOT BEING ABLE TO GET INTO THE BUSINESSES DUE TO THE SNOW PILED UP ON THE CURB/ STREET.	10/16/2017 4:08 PM

## Town of Eagle Downtown Parking Survey

12	Maximize use of private parking. I live next to Laughing Buddha and they always park in front of my house, rather than on her property because (I think) people don't know that space exists as parking. I do not have off-street parking, so am stuck if people choose to park in front of my house. As for work, in a similar fashion, parking that could be used for specific businesses is underutilized, leaving general street parking unavailable for those that need it.	10/16/2017 2:45 PM
13	Not a fan of restricting public parking to certain users (residents, employees, business patrons). It's all public. I am a fan of an outreach program to businesses to encourage them to encourage their employees to park in areas (like alleys behind the business) that does not take up potential customer/client space on main streets. Will also help if the Town can continue to finish parking and curb and gutter, or require adjacent property owners to do so with new development.	10/16/2017 1:22 PM
14	It's frustrating when residents park their cars for days and weeks at a time in places where workers would park (like nearer to Broadway)	10/16/2017 12:17 PM
15	I am on 4th street and people with business on Broadway will park on 4th all day so our customers and employee's have no place to park	10/16/2017 9:26 AM
16	People want to be so healthy here in Colorado but they also complain if they cannot park right in front of the front door. Really!!!	10/16/2017 8:02 AM
17	I typically don't find parking a problem in the Broadway area, but I tend to get to work really early-by 7:30.	10/16/2017 7:01 AM
18	I bank at Wells Fargo and it is hard to find space nearby.	10/15/2017 9:01 PM
19	I never see parking issues and I feel unless there are issues we can tackle larger issues	10/15/2017 8:42 PM
20	I don't think we currently having a parking issue; but if the town hopefully grows, gets more businesses and more restaurants, then I think there will be an issue to address.	10/15/2017 4:49 PM
21	I would like to see the Town of Eagle conduct a parking study for the area around the Eagle County Building, the Eagle Public Library and the Eagle Town Park, not just downtown Eagle. I would like the Town of Eagle to work with the Eagle County Gov. to alleviate some of the neighborhood "spill over" parking that is currently happening (neighbors are getting tired of it!) The County is not doing a good job of providing parking for their employees. I would like to see marked parking spaces all the way around the Town Park, with restrictions on overnight street parking that are enforced by the Police Department.	10/15/2017 1:54 PM
22	There are business owners that park all day on Broadway taking spots from paying customers. I would like to see that the town encourage these people to leave spaces open for paying customers	10/15/2017 12:20 PM
23	Roads need to be clear for plowing and cleaning.	10/15/2017 8:59 AM
24	It might be helpful to have time limits on Broadway and the immediate side streets in front of businesses such as the bank and restaurants. In other areas, around government buildings and residential areas, it is necessary to allow long-term parking.	10/15/2017 8:46 AM
25	There is not enough parking in the downtown area, this should have been a consideration when the area was revamped. Sometimes I just get frustrated and leave.	10/15/2017 6:38 AM
26	Planning and building of reserves for future town parking garage.	10/15/2017 6:20 AM
27	Parking is easy. Does not need to be fixed	10/14/2017 5:26 PM
28	The parking is confusing right now with the mix of angled and parallel parking. Also, there needs to be a decent amount of space for people loading large objects out of their cars for the business there as well as parents trying to put kids in carseats and strollers in the car.	10/14/2017 4:45 PM
29	Our downtown is a ghost town at night with plenty of parking but no affordable housing options. Old run down places. Need development to support more businesses with new housing options	10/14/2017 10:58 AM
30	With so many visitors to this area, I don't think a strong enforcement of parking restrictions without ample signage is a good impression of our town attitude. I would rather see those efforts put into direction and education for visitors.	10/14/2017 10:54 AM
31	I believe in having time limits for certain areas makes sense - long enough to enjoy the area without parking there all day. That being said, you need to have a place for employees to park, or we won't be able to have the services that we go there for. Would there be a way to time Broadway, and other areas in front of businesses, but leave other side streets and areas with out a time limit? There is no solution that is going to make everyone happy, so find the one that will keep everyone as safe as possible.	10/14/2017 10:36 AM

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32	Need housing options and with current parking codes that seems hard Creating tax revenue and more vibrant downtown	10/14/2017 10:30 AM
33	except at peak times, i don't encounter any parking issues. Seems like the town is doing a good job.	10/14/2017 10:23 AM
34	Need downtown development that incorporates more affordable options like 1 and 2 bedroom apartments	10/14/2017 10:22 AM
35	I park in Eagle frequently -- it's not much of an issue except that during peak day time hours you may have to walk a couple of blocks	10/14/2017 10:21 AM
36	Parking requirements for me depend on the weather, cold and miserable want to be as close to destination as possible but on beautiful days don't mind walking!!	10/14/2017 9:39 AM
37	Mostly go downtown to go to Wells Fargo. Many times there are no parking spots by the ATM entrance & even around the corner on Broadway.	10/14/2017 8:59 AM
38	The Red Canyon high school parking situation needs to be addressed. The students take up all the spaces within a 3 block radius around the school.	10/14/2017 8:54 AM
39	I don not see parking as an issue in Eagle. I do strongly support a Hwy 6 meaningful beautification program. The perception of Eagle would be much improved with a major beautification of hwy 6, this is where we should be spending our money, not on parking, parks and etc.	10/14/2017 7:42 AM
40	The town should purchase property as it becomes available no more than one block off of Broadway and convert it into a parking lot. This lot would be for employees and residents. this would open up Broadway for metered parking for businesses and offices. the offsite lot can also be available for overflow parking as well. as the CBD grows, there may need to be more than one satellite lot.	10/14/2017 6:45 AM
41	Question 3 is poorly conceived. NOBODY prefers to walk further and it should not be used to justify more on street parking. Instead, the valuable space in front of businesses should be used for expanding the scope of restaurants and coffee shops to allow customers an outdoor experience. Walking 2-3 blocks in a beautiful downtown (which we have) should not be perceived by board members as a negative.	10/13/2017 7:19 PM
42	Marked cycling lanes on pavement.	10/13/2017 6:04 PM
43	could increase parking of broadway by changing exiting parallel parking to pull in.	10/13/2017 5:06 PM
44	Please don't make the streets look like Avon! I work in Avon and their street/parking was crazy.	10/13/2017 2:57 PM
45	not mush to park for. Pharmacy, Hair salons, A couple of eating places and Town Hall	10/13/2017 2:52 PM
46	Forcing employees to park in certain areas would free up prime parking spots for visitors and customers alike.	10/13/2017 2:16 PM
47	No need appears to be indicating any reasons to make it complicated.	10/13/2017 2:13 PM
48	IMO It's all good as is don't change anything.	10/13/2017 1:38 PM
49	Signage is great - limits on time are tough (shop, coffee, hair cut, meet someone for lunch, bank - could easily be 5-6 hours and moving your car is annoying) - trailer/truck parking would be great - but I hate seeing reserved spots that are vacant and you can't park your vehicle there b/c it has a truck/trailer sign - residents should absolutely get to park near their homes, employees can be expected to have to walk so customers can get premier spots.	10/13/2017 12:50 PM
50	I don't have any issues with the current parking practices. I would hate to have to pay for parking or have time limits. Small towns in general should be more relaxed about these things. I think it would be a shame to bring in parking restrictions and strong enforcement. We are not downtown Denver.	10/13/2017 12:48 PM
51	Affordable housing Tap fees are too expensive for 1 and 2 bedroom units. No development will happen until tap fees and parking requirements are more reasonable	10/13/2017 12:37 PM
52	Less government interference is better	10/13/2017 12:30 PM
53	Business and town need infill development That would substantially increase sales tax but the parking requirements are too restrictive for CBD. No affordable housing for employees or teachers. Allow some 1 and 2 bedroom mixed use development to happen	10/13/2017 11:58 AM
54	have never noticed any issue.	10/13/2017 11:47 AM

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55	Current system seems to have worked for my personal needs over past ten years. Should (and hopefully so) more retail and restaurants be able to be attracted to the designated area parking flows should be reviewed.	10/13/2017 11:32 AM
56	Need to reduce CBD parking requirements to enable smart down town development to occur	10/13/2017 11:25 AM
57	I am in downtown quite often and do not have any problems with parking. If you are anticipating growth then perhaps address this but it's not a problem for me. I'm usually in this area beginning and end of work day and in the evenings.	10/13/2017 11:05 AM
58	Don't enable employee parking; support bus system. County employees should take the bus.	10/13/2017 11:04 AM
59	Lower parking requirements for commercial & residential	10/13/2017 10:59 AM
60	Just don't start charging for parking - ever	10/13/2017 10:48 AM
61	do slant parking all over. Parallel parking takes too much space and is hard to do for some	10/13/2017 10:34 AM
62	Employees should not take up close parking spots. Parking in front of business should be for customer parking (time limit ok) only.	10/13/2017 10:20 AM
63	Please don't install meters! Free parking is key to small town living.	10/13/2017 10:06 AM
64	Don't screw up the small town feel and accessibility with parking meters	10/13/2017 10:06 AM
65	As you know, there are several salons on Broadway. I work at Studio B, across from the Everything Store. If we have clients that are in for both color and cut, they are often in for more than two hours. Not to mention, if they would like to pop into another business to shop before or afterwards, they shouldn't have to worry about moving their cars. I actually had a client who was ticketed while having her hair done. She was in with me for 2 hours and 15 minutes. I felt horrible, and it left a very bad taste in her mouth towards the Town of Eagle. I am strongly against time limits, especially for clients/consumers. If employees had stickers for their cars, that would differentiate employees from consumers, and keep employees from taking up space from the people who are here spending money with us. Just a thought. Thanks!! Jill	10/13/2017 9:18 AM
66	n/a	10/13/2017 8:53 AM
67	The town has tons of parking in the downtown. And the countless times I've come downtown, I've literally never needed to park more than a half a block away from where I was going. Downtown's are not the place to have designated specialty parking. There's always enough parking for those folks, thought they may have to walk farther, but that's the deal when you're driving around a large trailer or something similar. The bigger challenge, which is addressed in the survey, is the town's parking standards. Eagle needs in filling it's downtown to make that area have any pizzazz, but it's never gonna happen with existing parking requirements.	10/13/2017 7:45 AM
68	time limits are not enforced on a daily basis and people park longer than time limits preventing customers to park on Broadway and businesses losing business	10/13/2017 6:18 AM
69	So sorry, we live in Bull Pasture, are retired and ALWAYS walk to the designated area so we've never experienced any issues.	10/12/2017 10:04 PM
70	Looking for all new residential development to include sufficient parking for its own tenants. If the town does start to draw people that try to do overnight parking in vans etc I would prefer we can enforce that out. Also in terms of parking for specialty vehicles, that's particularly on my mind because we have a neighbor who parks a ridiculous 18 wheeler across the street and fires it up before 5 in the morning. It's far louder than a passenger vehicle and shouldn't be parked on the street in a residential neighborhood.	10/12/2017 9:57 PM
71	I have not thought about 11 and 12, but I am sure that something needs to be done. I rarely am in downtown during the day, but when I am, that is when there is difficulty with parking, which I am sure has something to do with employee parking. Maybe designated areas for employee parking off of Broadway might open up spaces on Broadway. I would think that businesses would prefer to have open spaces in front of their entrances for possible customers, so maybe timed parking during the working hours.	10/12/2017 9:26 PM
72	What do you think we are?! Vail?! Pull it together Town Of Eagle.	10/12/2017 9:22 PM
73	it is fine don't mess with it. Do not add fees to park	10/12/2017 9:04 PM
74	We have many problems in Eagle. Parking is not one.	10/12/2017 8:54 PM

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75	I think parking time limits are completely unnecessary and would be a deterrent to downtown visitation. I live near a residence where an 18 wheeler with trailer is frequently parked and I do not think that is appropriate. I would support a policy of no long term truck parking in the downtown area.	10/12/2017 8:51 PM
76	Never in over 20 years of living in Eagle have I changed my behavior based on a perceived parking problem. Give people a compelling reason to visit downtown or attend an event and they'll find a way to make it happen. Also we need more/better bike racks and alternative options to driving. Not because there's a parking problem but because it's a better and more sustainable way to live	10/12/2017 8:15 PM
77	There is an overabundance of downtown parking; the grid works well for proximal parking displacement. If and when a parking shortage were ever realized, it encourages alternative forms of transport such as walking and biking, all of which works well in Eagle.	10/12/2017 8:00 PM
78	Would like to see some parklets to break up long groups of parking spaces.	10/12/2017 7:39 PM
79	Is there any room for a parking lot?	10/12/2017 7:37 PM
80	I live in between 4th and 5th street on Broadway and find it difficult to not be able to park overnight on Broadway. When I am home alone, the dark back alley can be threatening and parking under the street lights would be much safer. A residential parking permit would make it SO much easier!!	10/12/2017 7:32 PM
81	At present parking in the downtown does not appear to be a problem. I welcome the day that it is, that means we're doing something right!	10/12/2017 7:25 PM
82	Parking in that area is fine from my perspective. I think we need to only make changes that support the local businesses. Obviously, people need to respect residential parking - not blocking driveways but residential users should not have free range to park whatever they want (cars, trailers, campers, etc.) in the right-of-way. There isa lot of junk being stored out there.	10/12/2017 7:02 PM
83	I live a block away from where I work, so I usually don't park in this area for work, etc. I have private parking where I reside.	10/12/2017 6:51 PM
84	Striping would be fine. More regulations would not be welcome.	10/12/2017 6:42 PM
85	The students at Red Canyon High School are parking in excess of the two hour limit on Broadway.	10/12/2017 5:30 PM
86	I don't think there is currently a parking issue in Eagle. I don't think we should be trying to change anything at this point	10/12/2017 5:27 PM
87	I've lived in over a dozen cities, we don't have a parking issue and all of this energy can better be redirected towards stopping excessive spending.	10/12/2017 5:10 PM
88	When we must add additional parking lots away from the core area, long term parking such as employee parking should be there and close in parking reserved for customers and visitors. Businesses and employers need to be part of solving our community's parking problems.	10/12/2017 5:10 PM
89	Parking has been a problem for years. Your parking designation area is too small. You should expand it to include around the County Building, which is a large drain on parking. The County vehicles should be parked somewhere else allowing for visitor parking. Not county employees taking up large portions of the parking. Parking around the Eagle Public Library is taken up by County Employees taking space from Library patrons that include mothers with small children.. It would be nice if the TOE were proactive making some effort to talk with the County about the parking drain they cause... Parking around the Town Park is a problem. No police enforcement of parking signage. Why do we have it if they do not enforce it. People camping around the park with vehicles using TOE electricity and water... Come on TOE.... Really... This is a neighborhood. People in the Bull Pasture parking thier company vehicles around the town park because the parking restriction for the Bull pasture HOA... Do they think we want to see their broken down vehicles, wood chipperes and junky equipment around our neighborhood... Parking for TOE concerts in the park... inconsitent efforts by the police, if any effort is made at all.... Parking is out of control and affects the quality of the neighborhood and TOE environment... Glad you are doing a survey, but really expand the survey and do it right the first time.....	10/12/2017 4:48 PM
90	I think a small parking fee on Broadway and side streets would be fine. Residents would be excepted.	10/12/2017 4:27 PM
91	I live right outside the zone so usually walk or bike but have friends who live in the zone and want to continue to be able to park by their houses when needed.	10/12/2017 4:27 PM

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92	I find it more difficult to find parking from Wells Fargo north towards Red Canyon Cafe (both sides). It is easier to park further down past the Everything Store. If it is nice I don't mind but if it is cold and snowy I would prefer to park close to my destination. I wouldn't want Broadway to become a street with so many signs and paint stripes that it no longer looks like a nice little main street. I also think parts of Capitol Street could be better utilized for parking, like in front of the church.	10/12/2017 4:26 PM
93	Please stop plowing all of the snow on the corner of 3rd and Capitol, as this blocks all parking to my office within a large radius. Snow should be located other than this prime parking area.	10/12/2017 4:10 PM
94	Residents should not have to pay for a permit to park at their home. The county needs to find a better solution to all their cars - employees and county vehicles as they take up the vast majority of the parking along 5th and 6th from Broadway to Washington daily.	10/12/2017 4:10 PM
95	The two areas where I have trouble finding parking are in front of red canyon Cafe and the everything store. Otherwise, I can always find a spot.	10/12/2017 3:49 PM
96	I worked downtown for 3 years. I have also lived in eagle for over 20 years. I do not think we have a parking problem downtown! I think we have a vitality problem downtown. I never have a problem parking. I prefer to walk or ride my bike and parking has never deterred me from doing business downtown Eagle.	10/12/2017 3:35 PM
97	This survey is far to vague to give an opinion on a lot of what you are asking.	10/12/2017 3:09 PM
98	Continue to Pave and stripe more nearby ROW	10/12/2017 2:51 PM
99	I get my hair done at a shop at 4th & Broadway. The parking in front of the shop has a 2 hour limit. My hair takes 3 hours to get done, so we have to go out and move my car every time Eagle's code enforcement person marks my tires, so I don't get a ticket. That is ridiculous!! Why can't the shop have 2 spaces for their customers? I try to park on 4th street, but typically there are no spaces available. I can walk farther, but what about their elderly customers, especially in the winter time? We should not have to worry about getting a ticket when we are giving our business to your town!!! Sorry hair color and perms take 2 1/2 to 3 hours. Please give Mane Street Hair Design 2 spaces without time limits!!!	10/12/2017 2:49 PM
100	We like the current parking situation, and hope no changes are forthcoming. Future growth/development may necessitate changes to keep the situation satisfactory.	10/12/2017 2:30 PM
101	Current parking situation is good. Better awareness and delineation of public parking areas would be helpful. Enforcement of no long term parking of vehicles would help to keep spaces available in the future.	10/12/2017 2:24 PM
102	This is a bike heavy community. Time and money would be better spent encouraging bike commuting. Possibly a town bus in winter.	10/12/2017 2:21 PM
103	Broadway parking is a joke so are the resd. streets seems like any thing goes trucks, trailers semitrucks you name it	10/12/2017 2:10 PM
104	it seems that the high school students are taking up most of the available parking - build a parking lot for them. if it can't be close to the school then have a shuttle for them from one that is farther away	10/12/2017 1:43 PM
105	I've had no real problems parking in the study area. I DO however have issues with the restriction against street parking overnight in eagle ranch (i.e. Near the pool). Aside from snow removal in winter months (December through March) this is pointless and makes life difficult.	10/12/2017 1:43 PM
106	Outside of special events, such as parades or music, I never have difficulty parking around downtown Eagle, and even then, during those events, parking is available, but it may be a couple blocks away. At other times, I don't have any difficulty parking as close as I desire to my destination. I do not support setting up a permitting system or other restrictions to limit parking because we do not have the systems or draw in town to get people to Eagle if parking is not available. I see that as being detrimental to the businesses within downtown. I do support designating areas and providing wayfinding signage for larger vehicles such as RVs, vehicles with trailers, or delivery vehicles which can clog the streets if parking for these larger vehicles is not available.	10/12/2017 1:29 PM
107	I think eagle works well now and were a small community. Don't reinvent the wheel, just offer a bit more parking.	10/12/2017 1:22 PM

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108	Why are we exploring downtown parking without knowing what the communities goals for the downtown are? Regulations are a tool used to implement the vision/goals of a community. Why ask for input on best practices before understanding community perceptions on downtown parking challenges? Parking is a complex and technical matter. Very few folks will be able to provide an informed opinion on best practices related to best practices without being provided with some form of education on the topic. There also needs to be some community learning around how parking requirements impact the aesthetics/character and vibrancy/street life in the downtown. The community also needs to better understand how minimum parking requirements impact the feasibility of downtown development. Eagle has missed out on a number of beneficial downtown projects due to antiquated downtown parking requirements.	10/12/2017 1:18 PM
109	Don't turn the Town of Eagle into an overly governmental regulated zone. Need to keep the people friendly, community welcoming feeling that the town has been known for.	10/12/2017 1:17 PM
110	I believe there is a slight parking issue during business hours but I do not think it is that big of an issue to require residential parking permits or time limits.	10/12/2017 1:15 PM
111	ALLOW BRUSH CREEK VILLAGE RESIDENTS TO PARK ON THE STREET	10/12/2017 1:12 PM
112	I have never in 12 years of living here ever had any problem finding parking in relatively close proximity to my destination. The only issue I have is on church Sundays people park all over the place and do not obey signs, blocking sidewalks. But it's like for an hour or 2 so while it annoys me, I generally laugh it off. This is not something worth spending money or time on.	10/12/2017 1:06 PM
113	I don't support paying for parking in Eagle.	10/12/2017 1:05 PM
114	All those trailers and unregistered vehicles got to go!	10/12/2017 12:58 PM
115	Business should do there best to provide employee parking so as not to interfere with the closer parking for customers.	10/12/2017 12:57 PM
116	I work in this area and don't think there is an issue at all!	10/12/2017 12:56 PM
117	When it comes to enforcement, permits etc. It depends. 1st as long as there is ample.space to efficiently park and access commercial areas or park and ride bone yard or.whatever then I'm happy. If there are issue with parking in residential areas then permits would help that. Would it then stress the commercial.areas? Also enforcement of normal parking rules is good. If the town is considering metered parking. I am against that.	10/12/2017 12:52 PM
118	I think it's nice to have a limit on the parking spaces on Broadway but am seriously opposed to strict fines and enforcement. I already am appalled at how our town police bully people and harass law-abiding citizens. For the size town we have and the "friendly" environment in our town, the police officers here (in my limited but EVERY SINGLE experience) are extremely unfriendly, unneighborly, picking on people who contribute GREATLY to this community. I literally am terrified to get pulled over by a police officer in this town and that is a shame. So...in the ticketing department, because of my experience with power-hungry officers, I would be scared to have more enforcement of parking things. I really don't think it's that bad down there.	10/12/2017 12:52 PM
119	Huge numbers of ILLEGAL parking on a regular basis and no enforcement.( cars parked in no parking zones and resulting in seriously obstructed views of intersections )	10/12/2017 12:51 PM
120	I would like paved parking rather than dirt or gravel that exist in some areas for parking.	10/12/2017 12:49 PM
121	I don't feel that Eagle has a parking issue currently.	10/12/2017 12:38 PM
122	Pointless to have unenforced parking restrictions. Having the ability to provide permit parking for additional residential units would be a net benefit to the downtown core	10/12/2017 12:36 PM
123	Residences should have there own parking.	10/12/2017 12:35 PM
124	Frequently see many abandoned vehicles, trailers, etc. taking up parking spaces in the subject area for extended periods of time (weeks or even months). Limits on these should be enforced. There is ALWAYS ample parking within the subject area in my opinion. I've never been frustrated by a lack of parking, and walking a couple of blocks isn't an inconvenience.	10/12/2017 12:33 PM
125	Signage on Broadway is terrible. It's not clear that there is two-hour parking on all spaces. In fact, the signage appears as if there is only two-hour parking on the diagonal spaces and NOT on the parallel spaces. Enforcement is bad and seems to be different at times. I don't want enforcement or any limits on parking times.	10/12/2017 12:30 PM
126	For my situation i would like 1 designated parking spot in front of my house, with small walk way.	10/12/2017 12:26 PM

## Town of Eagle Downtown Parking Survey

127	The town does not have a parking problem to any great degree. People are just lazy. Successful downtowns have crowded parking.	10/12/2017 12:23 PM
128	Turning the main street of Broadway into a one way would allow for diagonal parking which I imagine would increase the number of parking spots. There should be designated areas for off Broadway employee parking to free up day use to shoppers, visitors, clients. With concepts of leasing housing parking requirements having residential "lot" or passes would also be suggested to keep track of how that change in small doses will impact Broadway.	10/12/2017 12:22 PM
129	Churn the existing in front spaces with time limits. Encourage the modest walk with no time limit spaces.	10/12/2017 12:20 PM
130	I park my vehicle as often as I bike or walk into downtown. Another pertinent question could have been: "How often do you park in the study area?"	10/12/2017 12:14 PM
131	I think we should do away with the 2 hour limit on Broadway. It's poorly marked in the first place. If it's going to be enforced there should be better signage. (i.e. signs on both sides of street).	10/12/2017 12:12 PM
132	Better walkable sidewalks. Not just in the newly constructed areas	10/12/2017 12:11 PM
133	only parking problems are on Broadway with angle parking	10/12/2017 12:09 PM
134	It is very inconvenient for someone who works on Broadway to be forced to move their vehicle every two hours.	10/12/2017 12:09 PM
135	The school takes up WAY too much daytime parking for no return to the community. That parking needs to be restricted.	10/12/2017 12:07 PM
136	We have a lot of issues with clients not being able to park in the immediate vicinity due to Red Canyon students parking during classes. No warnings or tickets are being issued as it is still an on-going problem. One instance is that a school bus was parked in front of our office for the majority of the day.	10/12/2017 12:07 PM
137	I don't generally have a problem finding a parking space on Broadway but, as Eagle grows, it's definitely going to become more of an issue.	10/12/2017 12:07 PM
138	I don't want it more complicated or restricted. I do, however, want there to be enough parking spaces as our town grows, so that the businesses in downtown can thrive and I can patronize them.	10/12/2017 12:03 PM
139	The diagonal parking on Broadway was a bad idea, dangerous when backing out into the street.	10/12/2017 12:02 PM
140	.	10/12/2017 11:55 AM
141	I have not had a difficult time finding parking when I go to Eagle. I am pretty much exclusively there on either nights or weekends.	10/12/2017 11:51 AM
142	I haven't ever perceived that we had a parking issue in this study area. It's not like I'm trying to find a place in downtown Denver. But I have to admit I'm not downtown that often, maybe once a month. Sometimes it's a 5 minute visit to the Pharmacy, other times it's a trip to the Brewery or the Sushi place. The Wells Fargo area seems to be a pain sometimes but I also think if Wells Fargo finally added a drive up ATM then the parking wouldn't be an issue.	10/12/2017 11:51 AM

Town of Eagle Downtown Parking Survey

Q14 What parking practices have you seen in other communities? In your opinion, would any work well for Eagle? (no character limit)

Answered: 90 Skipped: 278

#	RESPONSES	DATE
1	In my experience there is no need for parking restrictions in downtown Eagle (Broadway). In the neighborhood around the University of Denver, which only has fee-based parking, on-street parking is only allowed for one hour within 6 blocks of campus. Anyone who doesn't want to pay to park must hike from a ways away. Residents are allowed a permit to park on the street where they live. This seems to work well to regulate parking during the work day.	10/30/2017 10:07 PM
2	seriously you think eagle has a parking problem.. hah	10/27/2017 3:43 PM
3	Parking area designated for long term parking	10/25/2017 11:05 AM
4	Employees provided and encouraged to park in spots not attractive to customers/clients of businesses, Enforcing zoning to not include manufacturing (of bicycles) in the Central Business District. IN FACT just enforcing all zoning laws ...REALLY? Accessory dwelling units all over town?0?? Why have ANY zoning if it is not enforced? REALLY Eagle Is there any focus here?	10/24/2017 11:33 PM
5	bike share and or bike racks. Bike parking stalls like what has been done in Bozeman MT. Keep working on areas outside of downtown to allow easy access by walking and biking without the need for driving all the time.	10/24/2017 4:10 PM
6	.	10/24/2017 3:14 PM
7	This is very tough to do when you have residential and commercial in the same area. Every home owner seems to have a junk yard of crap that spills into our business district/parking spaces. If ppl were to store their campers, toppers, boats etc on their property and not on the street that would help	10/20/2017 9:41 AM
8	Down town Canon City was changed in the past few years. They increased parking along Main St. and the adjacent blocks.	10/18/2017 8:14 AM
9	Time limits	10/18/2017 12:40 AM
10	A parking structure for employee parking, thus opening up prime parking for customers and short term parking.	10/17/2017 8:08 AM
11	Improving public transportation and transportation alternatives (walking, biking, etc.) within communities to minimize the need for personal vehicles.	10/16/2017 2:45 PM
12	Don't try back-in diagonal parking! Haven't seen that work well at all.	10/16/2017 1:22 PM
13	I do not feel the parking at the south end of Broadway closer to the court house should be timed metered parking as nobody really parks there.	10/16/2017 9:26 AM
14	Backing out especially on Broadway is sometimes a little scary but only because people flying down the road like crazy. We are quickly becoming the gauntlet like Edwards where speeding is concerned. Why are so many people having to put there own purchased signs out in front of their homes to try and get people to slow down. Why are speeding issues being concerned?????	10/16/2017 8:02 AM
15	I like how the parking is restricted to 2 hours on Broadway during work hours, so people who want to support our local businesses have a place to park. Honestly, I don't see parking as an overall issue in downtown Eagle.	10/16/2017 7:01 AM
16	Some communities actually have parki g lots	10/15/2017 10:22 PM
17	Clearly marked parking spaces with time limits and restrictions on overnight parking. Businesses (Governments) that take responsibility for their employee parking by providing buses or carpools from other locations or Park-n-Rides. Permitted parking. Require business employees to walk further distances and reserve close up parking for visitors.	10/15/2017 1:54 PM
18	Ultimately, there should be no parking on certain days to allow people to congregate and shop.	10/15/2017 8:59 AM

## Town of Eagle Downtown Parking Survey

19	I work downtown and I don't see too many parking problems-yet. Perhaps if the town can ever attract businesses such as retail & dining to the empty buildings on Broadway that may change. Having more secure bike parking would be nice.	10/15/2017 8:46 AM
20	Parking garage and encouraging People to walk Main Street.	10/15/2017 6:20 AM
21	Ride bikes or use public transportation We don't have parking issues. Just because u live downtown doesn't mean u own the public parking in front of your home. Get some exercise and walk 5 spots to your destination	10/14/2017 10:58 AM
22	I have seen parking garages used in Denver and Edwards that would work here to utilize the space and keep the town center from sprawling into available housing space. I have also seen a planned graphic theme for directing people to parking from the road, this would allow people to find parking at the end of Broadway and enjoy a stroll through town, possibly developing the business center. I think the landscaping and street character of this area that is already in place helps promote this continuation of the town center into the Town of Eagle Park area and County Buildings.	10/14/2017 10:54 AM
23	Other MTN towns don't seems to have parking requirements but have more local businesses because people live in downtown core. Use of public transportation. 1 and 2 bedroom units tenants have bike and limited vehicles	10/14/2017 10:30 AM
24	Other communities seem to be getting rid of parking requirements for a more vibrant downtown. Not a big deal to walk a block or two. Creates tax revenue and helps solve housing issue	10/14/2017 10:22 AM
25	We're not like other communities and putting up additional restrictions and doling out tickets is not in the best interest of getting people to old downtown to dine and shop. Where issues sometimes do occur is high visitation areas like the county building and TOE offices -- but again, you just have to go a block or two away	10/14/2017 10:21 AM
26	I don't really have problems with parking most of the time; however I am pleased you are looking ahead as our little town is growing. Good luck!!	10/14/2017 9:39 AM
27	Limited time parking in Edwards. Don't really feel that is necessary & many business owners in Edwards say that the time limit isn't enforced anyway so why have it.	10/14/2017 8:59 AM
28	The practice of thinking ahead and not waiting until there's a problem.	10/14/2017 8:54 AM
29	Durango and Steamboat have satellite parking lots and restricted on street parking in their CBD.s and it works well. Aspen has a small parking structure in the Rio Grande building (town offices) that is a 3-4 block walk to town. there is a fee but it is used mostly by business folks when having meetings in town and by shoppers since it is less of a fee than on street parking	10/14/2017 6:45 AM
30	France, Spain, Holland: Parking within 20 feet of the fornt door is non-existent. This allows customers to park a few blocks away and SEE the town and browse a shopping opportunity on their pleasant walk (we love outdoors in Eagle!) to the restaurant, pharmacy, coffee shop, liquor store, vet, brewery or whatever else. I have also seen tremendous economic power in making the river and downtown an easily walkable experience.	10/13/2017 7:19 PM
31	What I love about Eagle is its small town feel, and that's why we moved here! I don't want to lose that feel by adopting strict parking regulations or turning our small town into something it isn't (or at least something I hope it doesn't become!)	10/13/2017 5:45 PM
32	Just being clear on what is ok and what isn't. Being friendly and welcoming as its this first place many come to upon coming into town. Some towns get too stiff and strick and it doesn't welcome people. We need to keep the hometown feeling warm even if it get really cold here.	10/13/2017 2:13 PM
33	??	10/13/2017 2:09 PM
34	leave it alone	10/13/2017 11:47 AM
35	Prior experience in Chicago, not a worthy example for ideas.	10/13/2017 11:32 AM
36	A lesson in what not to do: the changes in front of Brush Creek Elementary school, removing the neighbor parking on Eagle Ranch Road, removing a necessary crosswalk so now kids just cross mid street with no crosswalk and the poorly though out traffic flow that everyone ignores and just drives where they want. Please don't do anything like this downtown!!! I hope the town is reevaluating this problematic and unsafe change at Brush Creek Elem and including community members who use this street daily for input on how to correct these unfortunate changes.	10/13/2017 11:05 AM
37	There is wasted space at the end of Broadway where you could build more parking space at County Building for shoppers and diners.	10/13/2017 11:04 AM

## Town of Eagle Downtown Parking Survey

38	What doesn't work is parking meters! Those drive potential customers away from downtown local businesses.	10/13/2017 10:59 AM
39	If you do anything do a 2hr limit or something	10/13/2017 10:06 AM
40	More diagonal parking, rather than parallel	10/13/2017 8:53 AM
41	I think the most important thing you can see another downtowns is that the successful ones don't have a lot of offstreet parking. Once there is a real significant arcing challenge, the town hasn't even close to that right now, then it's time to start looking at structured parking or strategically located surface parking. Parking in our youth and behind buildings can help with this, but it's very important that appears to not have large surface parking along the street. It looks like diagonal parking is used in a lot of places to address some of these challenges as well.	10/13/2017 7:45 AM
42	Back-in angled parking.	10/12/2017 11:10 PM
43	No overnight parking and no semi truck parking seem to work well.	10/12/2017 9:57 PM
44	I mentioned them in 13.	10/12/2017 9:26 PM
45	adding parking fees, the obvious reason for this survey, would kill local businesses	10/12/2017 9:04 PM
46	Do people really think eagle has a parking problem?	10/12/2017 8:15 PM
47	Decoupling parking from condo / business purchase, reducing and/or eliminating parking requirements reduce unnecessary or arbitrary barriers to development along with direct autocentric subsidization.	10/12/2017 8:00 PM
48	Pedestrian only Main Street and move all parking to ends of Broadway.	10/12/2017 7:39 PM
49	A parking lot	10/12/2017 7:37 PM
50	Eventually perhaps metered parking	10/12/2017 7:25 PM
51	Other places have parking meters. they are a bad idea.	10/12/2017 6:51 PM
52	Require parking when new businesses are built.	10/12/2017 6:42 PM
53	Special district to raise funds for a parking lot.	10/12/2017 5:30 PM
54	We don't have a parking issue.	10/12/2017 5:10 PM
55	Yes, actual enforcement by the police on a consistent basis. An educational campaign trying to educate and seriously enforce the issues after the educational process... Takes some political back bone and effort... Seems like the council wants to only piss off certain folks and let the others just run rampant.... Where are the actual values of the TOE and how does the council actually see them living up to them... I have not seen it from this town in many many years....	10/12/2017 4:48 PM
56	Time limits with varied spans Metered parking Public parking lots Parking enforcement All of the above could possibly be beneficial to the Town	10/12/2017 4:37 PM
57	Lower parking ratio requirements. Promote pedestrian and bicycle activity.	10/12/2017 4:27 PM
58	In Denver I had a resident pass and only one guest pass. It was hard to have friends over for dinner, please don't do that here	10/12/2017 4:27 PM
59	I have seen the meter parking by number where you can use your bank or credit card to pay for a numbered spot during business hours (not nights or weekends). I am not sure I like this for Eagle. I wouldn't want to pay for parking. Perhaps having a few designated spots for specific businesses may work that are time limited say for banking or picking up food or coffee so that cars cycle through faster.	10/12/2017 4:26 PM
60	Parking lots 1 -2 blocks off the Main Street with signage and parking for visitors, customers. Designated parking for employees/owners of businesses without time limits. Please do not do what Breckenridge did-that system is horrible	10/12/2017 4:10 PM
61	Bike sharing, better/more prevalent bike racks. Make it easier for people to not have to drive downtown.	10/12/2017 3:35 PM
62	In Denver, street parking keeps speeds low. When cars are parked on the street, drivers navigate the street more slowly; the cars on the street serve as speed checks. When there is no parking on the street, drivers go faster because there's nothing blocking their way. In the Bluffs and Upper Kaibob, cars often travel well over the speed limit because the road is clear of any obstacles. Street parking slows traffic and makes neighborhoods safer for kids.	10/12/2017 2:57 PM

## Town of Eagle Downtown Parking Survey

63	Tickets for horrible parking jobs.	10/12/2017 2:54 PM
64	Too early for paid parking program on Broadway unless want it empty looking	10/12/2017 2:51 PM
65	No employee parking in prime spots	10/12/2017 2:37 PM
66	good question the riverpark sure wont help anything should spend money on up dates sewer,water steet drains sidewalks lights alley way ,signs	10/12/2017 2:10 PM
67	Parking structures	10/12/2017 1:43 PM
68	I've seen priority spots for low emissions vehicles, which I like, but hard to enforce.	10/12/2017 1:43 PM
69	1. In the words of Donald Shoup - "Where do minimum parking requirements come from? No one knows." Eliminate minimum parking requirements in the downtown. Successful downtowns are places designed for people, not cars. It is not local governments responsibility to ensure you have a place to park your car. 2. Allow downtown development to use on-street parking for parking needs. 3. Improve and increase on-street parking supply along downtown streets and Grand Avenue. 4. Better management of desirable on-street parking supply. In other words the parking supply along Broadway. Parking spaces don't free up if they're free and/or have no time limit. 5. Explore ways to improve ease of access to the downtown via bike, on foot, public transit, etc. There should be little reason for folks in Eagle to drive downtown. It's a small community and it should be easy and convenient to get downtown absent a vehicle.	10/12/2017 1:18 PM
70	Well positioned handicapped parking so those in need aren't forced to park far from destination. Generous spacing for lined parking spaces so not cramped or banging car doors into other vehicles. Perhaps an overflow parking area for local employees and those planning to spend a good part of the day.	10/12/2017 1:17 PM
71	Please don't refer to any parking practices that the Town of Vail uses.	10/12/2017 1:15 PM
72	Limit resident and worker parking on street spots	10/12/2017 1:11 PM
73	Eagle does not have a parking problem - don't create one by making it more difficult. Or figure out a way to add parking into the underused alleys for employees if you perceive there is a problem.	10/12/2017 1:06 PM
74	I think what we have now is working GREAT!	10/12/2017 1:05 PM
75	If possible a parking lot in close proximity to Downtown. As the town grows this will become more important.	10/12/2017 12:57 PM
76	N/A	10/12/2017 12:52 PM
77	Parking lots in the study area.	10/12/2017 12:42 PM
78	I worked in the River Walk for a while. Now there is a problem.... They started encouraging employees to avoid parking there and rewarded them with gift certificates that went into a weekly drawing. They encouraged car pools and such.	10/12/2017 12:38 PM
79	More diagonal parking where possible, town owned pay parking for overflow and revenue	10/12/2017 12:36 PM
80	Residential parking permits; pay-to-park stations; reasonable/reduced enforcement when tourists are in town	10/12/2017 12:34 PM
81	The off-street parking requirements in the downtown area need to be revisited if the town truly desires to activate the area. Consider having a fee-in-lieu program, and put the money towards formalizing more on-street parking or building a public parking lot, or even structure eventually. The town has more than adequate right-of-ways to accommodate more on-street parking.	10/12/2017 12:33 PM
82	I believe large parking lots on either or both ends of Broadway would be very beneficial. I have seen this is several communities and it works well.	10/12/2017 12:32 PM
83	I live in the Town of Vail and would HATE to see the Town of Eagle turn in to ToV. Eagle, in my opinion, rarely has parking issues in this downtown 'zone'. Only during special events, in which case the Eagle County building can be used for overflow parking in most cases and Brush Creek pavilion. People can walk. Do not need to have parking passes or strict enforcement here in Eagle. Please keep things simple in this town as most people I know do not think there is a parking problem that needs resolved. Thank you.	10/12/2017 12:30 PM
84	Enforce two hour parking on Broadway only	10/12/2017 12:23 PM
85	Off main street, Broadway in this case, Employee parking. Designated Load/Unload and hours for larger delivery trucks.	10/12/2017 12:22 PM

## Town of Eagle Downtown Parking Survey

86	Consistent stripping of parking spots and better signage.	10/12/2017 12:14 PM
87	Less landscape/streetscape creating more parking.	10/12/2017 12:09 PM
88	Definitely parking permits, tickets, education to the Red Canyon students, time limits, possible parking meters.	10/12/2017 12:07 PM
89	Having enough parking, really is the main thing. Although I do bike a lot, the idea that more can/will bike is unrealistic a lot of the year with weather and families. And pushing the biking instead of increasing parking spaces as needed will only hurt the downtown core. Encourage biking but always have ample, convenient parking.	10/12/2017 12:03 PM
90	.	10/12/2017 11:55 AM

Town of Eagle Downtown Parking Survey

Q15 In what zip code do you live? (optional)

Answered: 319 Skipped: 49

#	RESPONSES	DATE
1	81631	10/30/2017 10:08 PM
2	81631	10/28/2017 6:55 PM
3	81631	10/27/2017 3:43 PM
4	81631	10/25/2017 11:05 AM
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11	81631	10/23/2017 6:39 AM
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20	81637	10/17/2017 8:36 PM
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34	81631	10/16/2017 2:42 PM
35	81631	10/16/2017 1:29 PM

## Town of Eagle Downtown Parking Survey

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41	81631	10/16/2017 9:27 AM
42	81631	10/16/2017 8:03 AM
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44	81631	10/16/2017 7:02 AM
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## Town of Eagle Downtown Parking Survey

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## Town of Eagle Downtown Parking Survey

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199	81631	10/12/2017 7:03 PM

## Town of Eagle Downtown Parking Survey

200	81631	10/12/2017 6:51 PM
201	81631	10/12/2017 5:54 PM
202	81637	10/12/2017 5:48 PM
203	81631	10/12/2017 5:31 PM
204	81637	10/12/2017 5:27 PM
205	81631	10/12/2017 5:20 PM
206	81631	10/12/2017 5:11 PM
207	81631	10/12/2017 5:10 PM
208	81631	10/12/2017 5:05 PM
209	81631	10/12/2017 4:49 PM
210	81631	10/12/2017 4:43 PM
211	81637	10/12/2017 4:37 PM
212	81631	10/12/2017 4:27 PM
213	81631	10/12/2017 4:27 PM
214	81637	10/12/2017 4:26 PM
215	81631	10/12/2017 4:10 PM
216	81631	10/12/2017 4:10 PM
217	81631	10/12/2017 3:55 PM
218	81631	10/12/2017 3:50 PM
219	81631	10/12/2017 3:47 PM
220	81631	10/12/2017 3:35 PM
221	81631	10/12/2017 3:32 PM
222	81631	10/12/2017 3:27 PM
223	I moved a year ago and now live in 29585	10/12/2017 3:26 PM
224	81631	10/12/2017 3:09 PM
225	81631	10/12/2017 3:00 PM
226	81631	10/12/2017 2:57 PM
227	81631	10/12/2017 2:54 PM
228	81631	10/12/2017 2:51 PM
229	81631	10/12/2017 2:49 PM
230	81637	10/12/2017 2:39 PM
231	81631	10/12/2017 2:37 PM
232	81631	10/12/2017 2:33 PM
233	81631	10/12/2017 2:30 PM
234	81632	10/12/2017 2:24 PM
235	81631	10/12/2017 2:24 PM
236	81631	10/12/2017 2:22 PM
237	81631	10/12/2017 2:21 PM
238	81631	10/12/2017 2:10 PM
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240	81631	10/12/2017 2:05 PM

## Town of Eagle Downtown Parking Survey

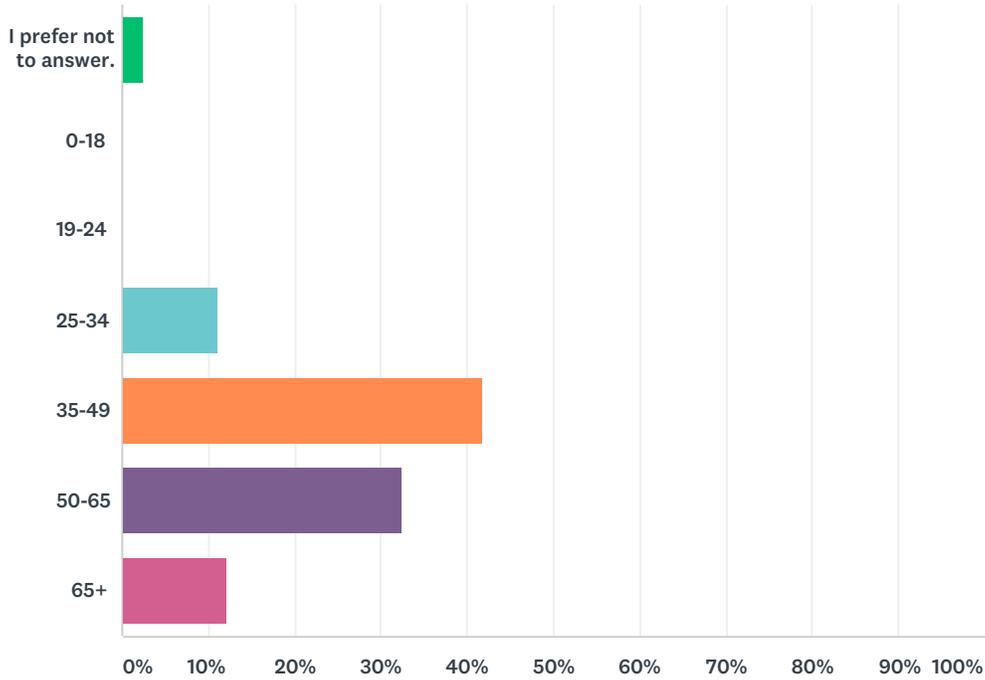
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249	81631	10/12/2017 1:36 PM
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252	81620	10/12/2017 1:29 PM
253	81631	10/12/2017 1:27 PM
254	81631	10/12/2017 1:23 PM
255	81637	10/12/2017 1:22 PM
256	81631	10/12/2017 1:19 PM
257	81631	10/12/2017 1:18 PM
258	81631	10/12/2017 1:17 PM
259	81631	10/12/2017 1:16 PM
260	81631	10/12/2017 1:13 PM
261	81631	10/12/2017 1:12 PM
262	81631	10/12/2017 1:11 PM
263	81631	10/12/2017 1:07 PM
264	81631	10/12/2017 1:06 PM
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273	81631	10/12/2017 12:56 PM
274	81631	10/12/2017 12:52 PM
275	81631	10/12/2017 12:52 PM
276	81631	10/12/2017 12:51 PM
277	81631	10/12/2017 12:50 PM
278	81631	10/12/2017 12:49 PM
279	81631	10/12/2017 12:48 PM
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## Town of Eagle Downtown Parking Survey

282	81631	10/12/2017 12:37 PM
283	81631	10/12/2017 12:36 PM
284	81631	10/12/2017 12:35 PM
285	81631	10/12/2017 12:33 PM
286	81631	10/12/2017 12:32 PM
287	81657	10/12/2017 12:31 PM
288	81631	10/12/2017 12:27 PM
289	81632	10/12/2017 12:24 PM
290	81631	10/12/2017 12:23 PM
291	81631	10/12/2017 12:23 PM
292	81631	10/12/2017 12:23 PM
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312	81631	10/12/2017 11:52 AM
313	81631	10/12/2017 11:52 AM
314	81637	10/12/2017 11:51 AM
315	81631	10/12/2017 11:49 AM
316	81601	10/11/2017 3:58 PM
317	81637	10/11/2017 1:33 PM
318	81620	10/10/2017 2:05 PM
319	81620	10/5/2017 5:54 PM

## Q16 What is your age? (optional)

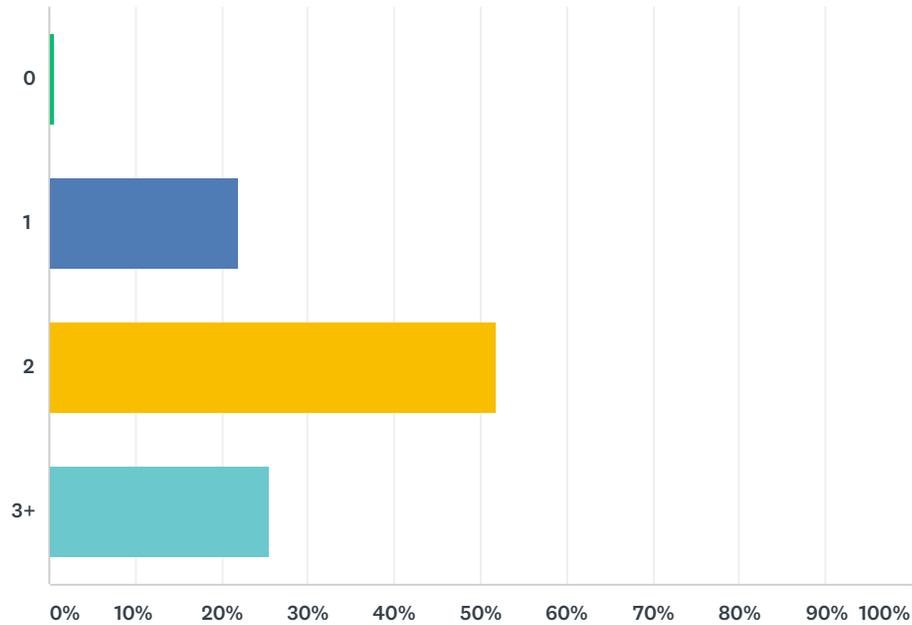
Answered: 332 Skipped: 36



ANSWER CHOICES	RESPONSES	
I prefer not to answer.	2.41%	8
0-18	0.00%	0
19-24	0.00%	0
25-34	11.14%	37
35-49	41.87%	139
50-65	32.53%	108
65+	12.05%	40
<b>TOTAL</b>		<b>332</b>

## Q17 How many vehicles do you own? (optional)

Answered: 328 Skipped: 40



ANSWER CHOICES	RESPONSES	
0	0.61%	2
1	21.95%	72
2	51.83%	170
3+	25.61%	84
TOTAL		328





## SALES TAX ALLOCATION

Financing via sales tax allocation requires that a certain percentage of municipal sales tax revenue be allocated specifically to a particular expenditure (in this case, operation, maintenance, and capital expense repayment for the proposed parking structure).

In many cases, jurisdictions (including both municipalities and counties) choose to implement this financing option through an increase in sales tax via voter referendum (e.g. a “one-cent” or “penny” sales tax increase). This method ensures that an allocation of sales tax revenue will not impact projects and services already being funded by this revenue source. Such “one-cent” sales tax allocations are commonly tied to infrastructural improvements, though in general potential uses are broader than parking alone, and may include public park maintenance, transportation infrastructure improvements like road widening, sidewalk or bike lane installation, or utility improvements. Lake County, Florida has successfully passed a voter referendum for a one-cent sales tax allocated towards a variety of transportation, parking, utility, and community resource improvements three times- in 1988, in 2001, and in 2015.

Other communities have leveraged business sales by increasing tax levy on the gross receipts of a business within a particular district while maintaining consumer sales tax rates, thereby avoiding a voter referendum. WALKER located one successful example of this mechanism in Alhambra, California.

### ALHAMBRA, CALIFORNIA

<b>Population Size</b>	84,782
<b>Median Income</b>	\$53,582
<b>Car Ownership</b>	96.5%

Downtown businesses in Alhambra, California are assessed an additional tax based on gross receipts, used to pay for operations, maintenance, and capital costs for downtown parking lots, and support beautification and aesthetic projects in the district. The figures below (Figures 11 and 12) show the district map in which the fee is levied, and the public parking structures and lots made available to businesses within the district.

Figure 4: Parking Sales Tax Area Boundaries—Alhambra, CA

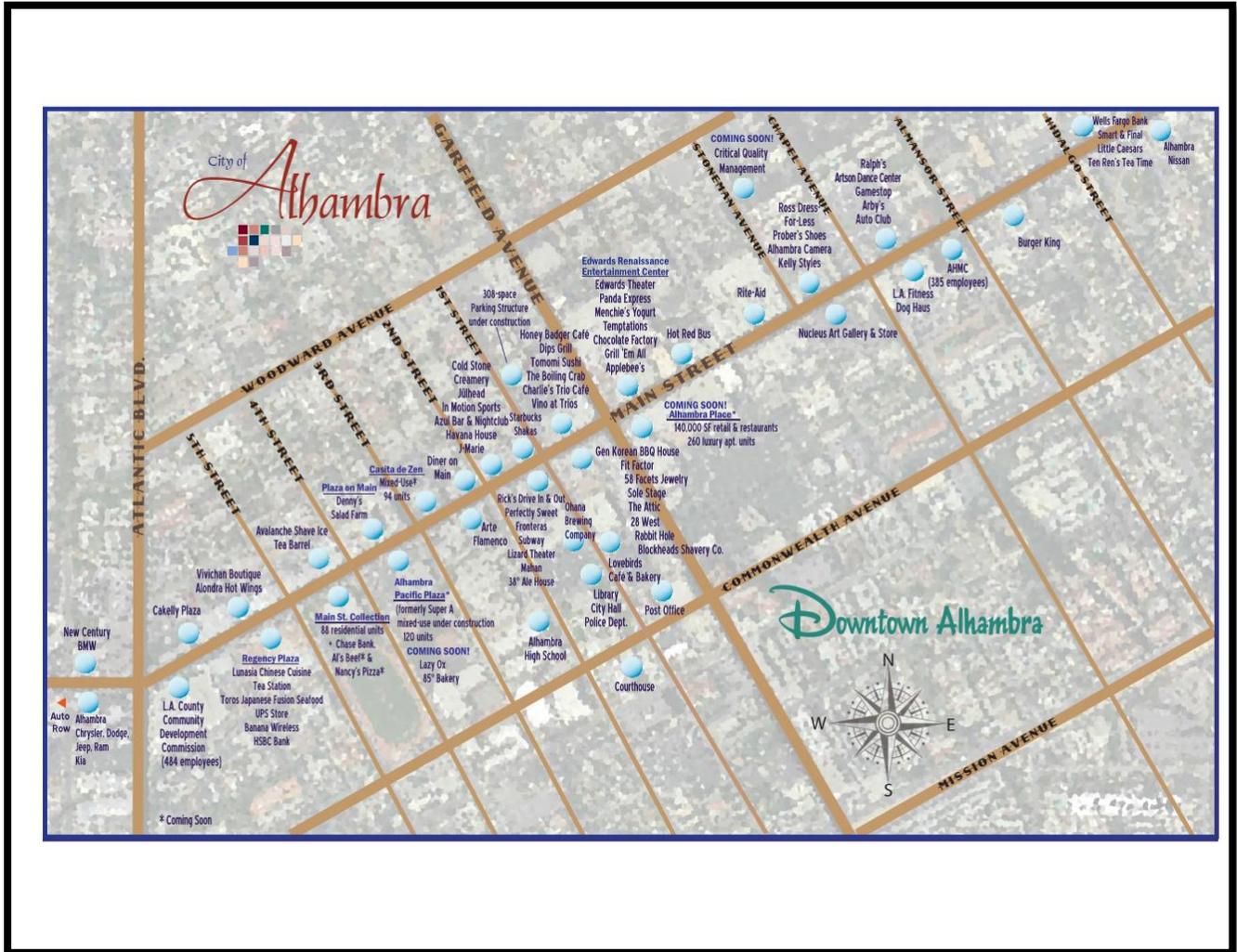
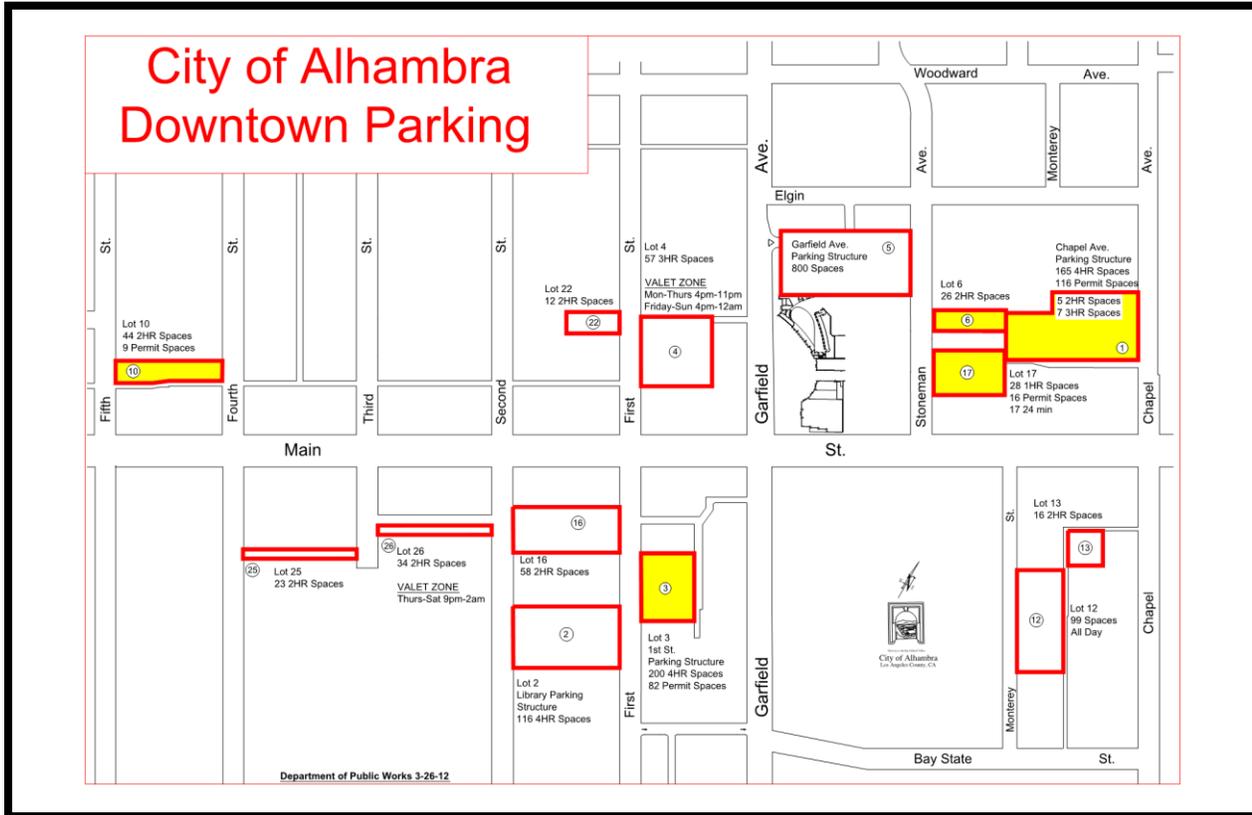


Figure 5.5: Downtown Parking Facilities—Alhambra, CA



### DEVELOPER/BUSINESS/OCCUPIER FEE

This financing structure generates revenue through a fee imposed on developers, businesses, or other occupiers in exchange for the use of public parking stalls. Such public stalls can be “credited” towards fulfilling off-street parking requirements in lieu of constructing dedicated parking.

Parking in-lieu fees, a similar revenue mechanism for public parking infrastructure, allow, encourage, or require that developers pay a fee to the city in lieu of constructing private parking, and are a more common financing structure than a developer/business/occupier fee (see above). However, this fee structure does not explicitly associate fees with infrastructure replacement costs, as is typically the case with a parking in-lieu fee. Within these parameters, WALKER located one successful case study in Pasadena, California.

### PASADENA, CALIFORNIA

<b>Population Size</b>	26,806
<b>Median Income</b>	\$66,384
<b>Car Ownership</b>	96.8%

The Town of Pasadena, California implemented the Zoning Credit Parking Program in its historic district, Old Pasadena, in order to enable developers, business owners, and other property owners to provide parking for their users while discouraging (and, in fact, disallowing) any construction of new parking. The program enables shared use (not exclusive use) of public parking spaces located in existing public parking structures; the number of spaces allowed per use is determined based on minimum parking requirements set forth in the City of Pasadena Zoning Code (Article 4). An initial \$738 application processing fee is required to join the program, followed by annual payments of \$250 per space. Processing requires sign-off by the City’s planning, transportation, and building and safety departments; the full application has been included as an attachment.

According to representatives from the City’s Parking Division, the program has met its key objective of fulfilling parking demand for this vibrant community while replacing the need for private parking infrastructure. In financial terms, the program contributes modestly to the operations and maintenance costs associated with the public parking structures used for the program. In 2016, the program yielded \$489,000, or 15% of total annual O&M cost for the structures.

### **BUSINESS IMPROVEMENT DISTRICTS**

Some municipalities and county governments use business improvement districts (“BIDs”) and parking tax districts as a means to generate income to fund parking facility capital improvements and operating expenses. Both business improvement districts and parking tax districts can be used to finance the acquisition of land; the construction, operation, and maintenance of surface parking lots and parking structures; as well as the costs of engineers, attorneys and other professionals needed to complete infrastructure projects. While Eagle has attempted to launch a business improvement district previously with limited success, it may be a viable option as the downtown continues to grow and thrive.

Over 1,200 BIDs have been implemented in the U.S. BIDs, which are most often formed at the request of their member businesses, typically address a wide variety of issues, not all related to parking. Common issues addressed include marketing, transit, beautification, signage, lighting, parking, street and public space maintenance, unarmed security patrols, “customer service representatives” or “ambassadors” to provide information and assistance to tourists and shoppers, etc. The collection of assessments tend to be applied uniformly on a square foot, gross receipts, or assessed value basis because benefits are universally recognized by all property owners. Typically, no exemptions or tax credits are provided to property owners who provide all or a portion of their required parking.

A smaller number of communities have implemented parking tax districts, which are more narrow in focus. WALKER identified two active, long-standing examples of these districts- one in Olympia, Washington and another in Tualatin, Oregon- as case studies for this financing option.

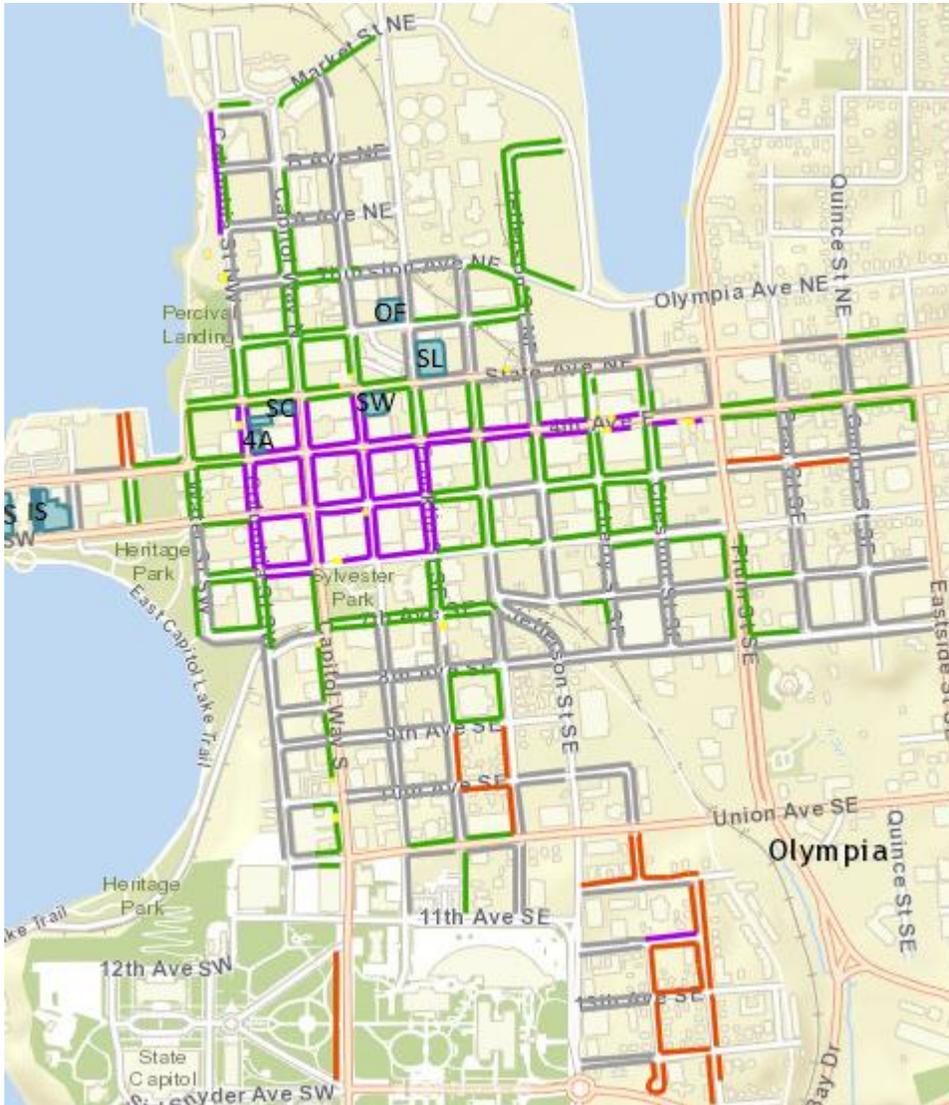
### **OLYMPIA, WASHINGTON**

<b>Population Size</b>	48,941
<b>Median Income</b>	\$53,617
<b>Car Ownership</b>	96.0%

The City of Olympia, Washington enacted a Parking and Business Improvement District in 2006. The District, encompassing the entirety of the City's downtown, levies special assessments to fund downtown parking improvements, including administrative costs, construction, operation, and maintenance costs. The assessments also fund a number of other aesthetic and beautification projects throughout the District.

The following figure shows an overview of parking within the district boundaries; metered areas are highlighted in yellow, purple, green, and grey; time-limited areas are shown in orange; and parking facilities are lettered and highlighted in blue. In addition to maintenance and operation of existing facilities and existing and future capital expenditures, the special assessments have also paid for an "Oly Smartcard" program, which retrofits the City's outdated coin-operated meters to be paid for with a debit "smartcard" purchased from the City.

Figure 1: Parking Business Improvement District—Olympia, WA



Assessments are levied upon business owners and multi-family residential owners/operators within the district boundaries. They are calculated annually based on the property’s use and intensity. The following figure is an assessment matrix showing how a particular property’s annual assessment is determined.

Figure 2: Parking Business Improvement Area Assessment Schedule

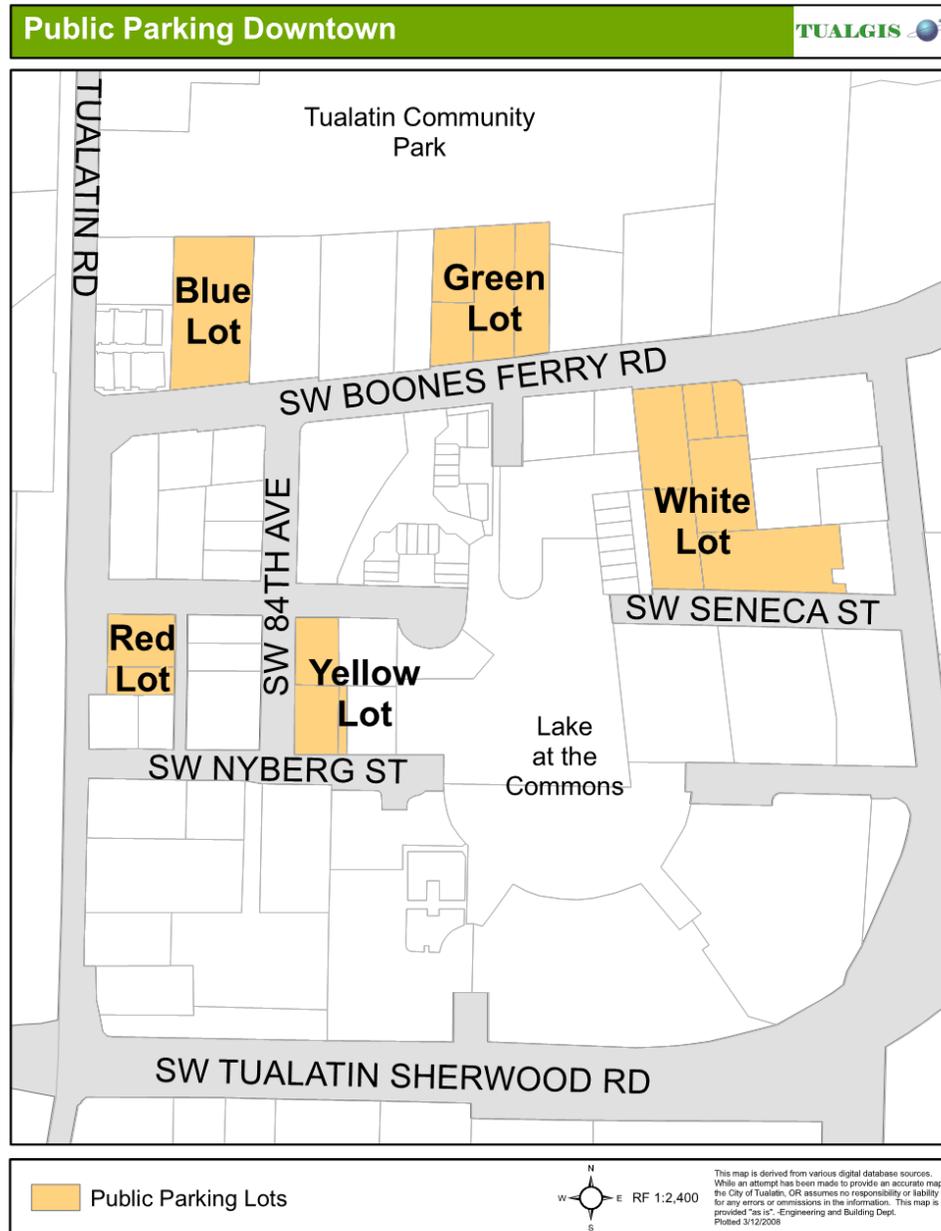
Business Type	Zone A			Zone B			Zone C		
	Large	Medium	Small	Large	Medium	Small	Large	Medium	Small
Restaurant/Retail	\$750	\$500	\$250	\$600	\$400	\$200	\$300	\$200	\$150
Professional/Service	\$400	\$300	\$200	\$300	\$200	\$150	\$200	\$150	\$150
Financial Institutions	\$750			\$600			\$300		
Lodging/Apartments	30 or less rooms = \$200; 31-50 rooms = \$300; 51+ rooms - \$400								
Personal Care Services	Minimum \$150 plus \$75 per station above 2 stations with a cap of \$500								

### TUALATIN, OREGON

Population Size	26,806
Median Income	\$66,384
Car Ownership	96.8%

The City of Tualatin, Oregon established its Core Area Parking District in 1979; fees levied by the district have supported construction, operations, and maintenance of the district's public parking facilities, paid for parking analysis and studies, and aided in other functions aimed to ensure efficiency of the district's parking system. The District covers 24 acres of downtown property, in which there are five public surface lots with 386 spaces in total. The following figure provides an overview of the district and shows the locations of the surface lots.

Figure 3: Core Area Parking District Downtown Parking



The District’s activities are overseen by a Board comprised of five members, each required to be an owner of a business located within the District boundaries. This Board serves in an advisory capacity to City Council, aiding in decisions regarding the location and design of new parking facilities, type and scope of improvements, amount of fees and taxes levied, and other policy and procedural matters.

The annual tax levied for a particular land use in the district is computed by multiplying the use’s gross leasable area (GLA) by the minimum number of spaces required for that land use (space factor) by the annual tax rate



(\$170 in 2016), then dividing the resulting number by 1,000. For example, a restaurant with 15,000 square feet of GLA would use the following calculation:

$$\begin{aligned}\text{Annual Tax} &= 15,000 * 5.00 \text{ (space factor)} * 170 / 1,000 \\ \text{Annual Tax} &= \$12,750\end{aligned}$$