

November 19, 2025

Robert Yagusesky
McDonald's USA, LLC.
110 North Carpenter Street
Chicago, IL 60607

Re: McDonald's Eagle
Trip Generation Comparison Letter
Eagle, Colorado

Dear Mr. Yagusesky:

This letter documents the results of a trip generation comparison analysis for the proposed McDonald's restaurant to replace a previously existing and standing Burger King restaurant located on the northwest corner of Interstate 70 (I-70) Westbound Ramps and Eby Creek Road intersection in Eagle, Colorado. The project is specifically located at 295 Eby Creek Road.

SITE CHARACTERISTICS AND ACCESS

The building area of the McDonald's restaurant is proposed to increase from 3,574 square feet to 4,070 square feet. A conceptual site plan of the project is attached. Access is currently provided and will continue to be provided by a full movement access located on the west leg of the Market Street and Eby Street roundabout intersection.

TRIP GENERATION COMPARISON

Site-generated traffic estimates are determined through a process known as trip generation. Rates and equations are applied to the proposed land use to estimate traffic generated by the development during a specific time interval. The acknowledged source for trip generation rates is the *Trip Generation Manual*¹ published by the Institute of Transportation Engineers (ITE). ITE has established trip rates in nationwide studies of similar land uses.

For this study, the same land use code applies to both the existing and the proposed buildings. Kimley-Horn used the ITE Trip Generation Manual average rate equations that apply to Fast-Food Restaurant with Drive-Through Window (ITE Land Use Code 934) for traffic associated with this development. The following **Table 1** summarizes the estimated trip generation for the proposed use versus the previous land use (calculations attached).

¹ Institute of Transportation Engineers, *Trip Generation Manual*, Twelfth Edition, Washington DC, 2025.

Table 1 – McDonald's Eagle Traffic Generation Comparison

Land Use and Size	Weekday Vehicles Trips						
	Daily	AM Peak Hour			PM Peak Hour		
		In	Out	Total	In	Out	Total
Previously Existing Land Use- Burger King (3,574 SF) (ITE 934)							
Baseline Trips	1,602	61	58	119	59	54	113
Total New Trips after Pass-by	722	31	28	59	27	24	51
Proposed Land Use- McDonald's (4,070 SF) (ITE 934)							
Baseline Trips	1,824	69	66	135	67	62	129
Total New Trips after Pass-by	822	35	32	67	30	28	58
Net Difference in Total Trips	+100	+4	+4	+8	+3	+4	+7

As shown in the table and based on ITE Trip Generation calculations, the proposed McDonald's is expected to generate approximately 822 weekday daily new trips, with 67 of these new trips occurring during the morning peak hour and 58 of these trips occurring during the afternoon peak hour. In comparison, the previously existing Burger King is calculated to previously be generating approximately 722 weekday daily new trips, with 59 and 51 of these new trips occurring during the morning and afternoon peak hour, respectively. Based on this analysis and trips being calculated based on building area, the proposed fast-food restaurant is calculated to generate 100 additional daily new trips, eight (8) more trips in the morning peak hour, and seven (7) more trips during the afternoon peak hour compared to the previously existing fast-food restaurant in the same development area.

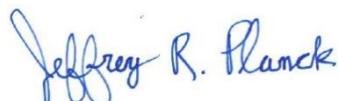
CONCLUSIONS

The current proposal, although the same land use type, is calculated to generate 100 additional daily new trips, eight (8) more new trips in the morning peak hour, and seven (7) more new trips during the afternoon peak hour compared to the previously existing fast-food restaurant in the same development area. The increase in trips is due to the building area increasing by 496 square feet and building area being the variable utilized in calculating trip generation for fast-food restaurants. Based on the land use remaining the same, it is conceivable that two fast-food restaurants with similar building areas could generate the same number of trips independent of building areas. Regardless, even if it is believed that trips could increase based on building area, the increase of eight (8) new trips during the peak hour equates to approximately one additional trip every seven (7) minutes. Kimley-Horn and Associates believes the project will be successfully incorporated into the existing roadway network.

If you have any questions or require anything further, please feel free to call me at (720) 943-9962.

Sincerely,

KIMLEY-HORN AND ASSOCIATES, INC.



Jeffrey R. Planck, PE
Project Traffic Manager





Trip Generation Worksheets

Existing Burger King Daily Trip Generation Calculations

	TRIP GENERATION CHARACTERISTICS					DIRECTIONAL DISTRIBUTION		BASELINE TRIPS			PASS-BY CAPTURE		NEW EXTERNAL VEHICLE TRIPS			
	Land Use + Code	Source	Scale	ITE Unit	Equation/Rate	Entering %	Exiting %	In	Out	Total	Rate	PB Trips	In	Out	Total	
1	(934) Fast-Food Restaurant with Drive-Through Window	ITE 12th Ed	3.57	KSF	$T = 448.12(X)$	50%	50%	801	801	1,602	55.0%	881	360	361	721	
								Total:	801	801	1,602	55.0%	881	360	361	721

Existing Burger King AM Peak Hour Trip Generation Calculations

	TRIP GENERATION CHARACTERISTICS					DIRECTIONAL DISTRIBUTION		BASELINE TRIPS			PASS-BY CAPTURE		NEW EXTERNAL VEHICLE TRIPS			
	Land Use + Code	Source	Scale	ITE Unit	Equation/Rate	Entering %	Exiting %	In	Out	Total	Rate	PB Trips	In	Out	Total	
1	(934) Fast-Food Restaurant with Drive-Through Window	ITE 12th Ed	3.57	KSF	$T = 33.24(X)$	51%	49%	61	58	119	50.0%	60	31	28	59	
								Total:	61	58	119	50.0%	60	31	28	59

Existing Burger King PM Peak Hour Trip Generation Calculations

	TRIP GENERATION CHARACTERISTICS					DIRECTIONAL DISTRIBUTION		BASELINE TRIPS			PASS-BY CAPTURE		NEW EXTERNAL VEHICLE TRIPS			
	Land Use + Code	Source	Scale	ITE Unit	Equation/Rate	Entering %	Exiting %	In	Out	Total	Rate	PB Trips	In	Out	Total	
1	(934) Fast-Food Restaurant with Drive-Through Window	ITE 12th Ed	3.57	KSF	$T = 31.6(X)$	52%	48%	59	54	113	55.0%	62	27	24	51	
								Total:	59	54	113	55.0%	62	27	24	51

Proposed McDonald's Daily Trip Generation Calculations

TRIP GENERATION CHARACTERISTICS					DIRECTIONAL DISTRIBUTION		BASELINE TRIPS			PASS-BY CAPTURE		NEW EXTERNAL VEHICLE TRIPS				
	Land Use + Code	Source	Scale	ITE Unit	Equation/Rate	Entering %	Exiting %	In	Out	Total	Rate	PB Trips	In	Out	Total	
1	(934) Fast-Food Restaurant with Drive-Through Window	ITE 12th Ed	4.07	KSF	$T = 448.12(X)$	50%	50%	912	912	1,824	55.0%	1,003	410	411	821	
								Total:	912	912	1,824	55.0%	1,003	410	411	821

Proposed McDonald's AM Peak Hour Trip Generation Calculations

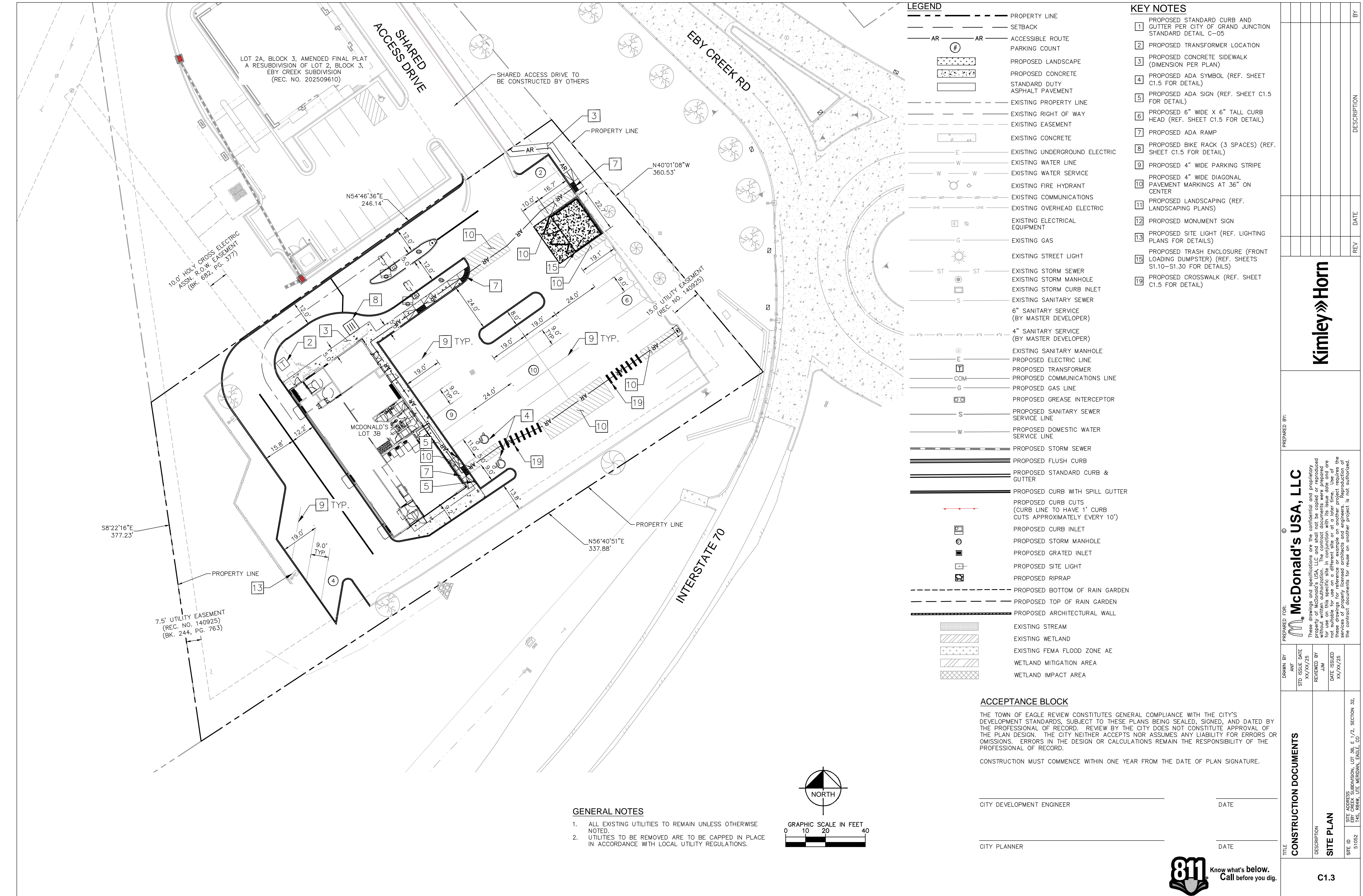
TRIP GENERATION CHARACTERISTICS					DIRECTIONAL DISTRIBUTION		BASELINE TRIPS			PASS-BY CAPTURE		NEW EXTERNAL VEHICLE TRIPS				
	Land Use + Code	Source	Scale	ITE Unit	Equation/Rate	Entering %	Exiting %	In	Out	Total	Rate	PB Trips	In	Out	Total	
1	(934) Fast-Food Restaurant with Drive-Through Window	ITE 12th Ed	4.07	KSF	$T = 33.24(X)$	51%	49%	69	66	135	50.0%	68	35	32	67	
								Total:	69	66	135	50.0%	68	35	32	67

Proposed McDonald's PM Peak Hour Trip Generation Calculations

TRIP GENERATION CHARACTERISTICS					DIRECTIONAL DISTRIBUTION		BASELINE TRIPS			PASS-BY CAPTURE		NEW EXTERNAL VEHICLE TRIPS				
	Land Use + Code	Source	Scale	ITE Unit	Equation/Rate	Entering %	Exiting %	In	Out	Total	Rate	PB Trips	In	Out	Total	
1	(934) Fast-Food Restaurant with Drive-Through Window	ITE 12th Ed	4.07	KSF	$T = 31.6(X)$	52%	48%	67	62	129	55.0%	71	30	28	58	
2																
								Total:	67	62	129	55.0%	71	30	28	58



Conceptual Site Plan



now what's below.
Call before you dig.

C1.3