

November 19, 2025

Robert Yagusesky
McDonald's USA, LLC.
110 North Carpenter Street
Chicago, IL 60607

Re: McDonald's Eagle
Trip Generation Comparison Letter
Eagle, Colorado

Dear Mr. Yagusesky:

This letter documents the results of a trip generation comparison analysis for the proposed McDonald's restaurant to replace a previously existing and standing Burger King restaurant located on the northwest corner of Interstate 70 (I-70) Westbound Ramps and Eby Creek Road intersection in Eagle, Colorado. The project is specifically located at 295 Eby Creek Road.

SITE CHARACTERISTICS AND ACCESS

The building area of the McDonald's restaurant is proposed to increase from 3,574 square feet to 4,070 square feet. A conceptual site plan of the project is attached. Access is currently provided and will continue to be provided by a full movement access located on the west leg of the Market Street and Eby Street roundabout intersection.

TRIP GENERATION COMPARISON

Site-generated traffic estimates are determined through a process known as trip generation. Rates and equations are applied to the proposed land use to estimate traffic generated by the development during a specific time interval. The acknowledged source for trip generation rates is the *Trip Generation Manual*¹ published by the Institute of Transportation Engineers (ITE). ITE has established trip rates in nationwide studies of similar land uses.

For this study, the same land use code applies to both the existing and the proposed buildings. Kimley-Horn used the ITE Trip Generation Manual average rate equations that apply to Fast-Food Restaurant with Drive-Through Window (ITE Land Use Code 934) for traffic associated with this development. The following **Table 1** summarizes the estimated trip generation for the proposed use versus the previous land use (calculations attached).

¹ Institute of Transportation Engineers, *Trip Generation Manual*, Twelfth Edition, Washington DC, 2025.

Table 1 – McDonald's Eagle Traffic Generation Comparison

| Land Use and Size | Weekday Vehicles Trips | | | | | | |
|--|------------------------|--------------|-----|-------|--------------|-----|-------|
| | Daily | AM Peak Hour | | | PM Peak Hour | | |
| | | In | Out | Total | In | Out | Total |
| Previously Existing Land Use- Burger King (3,574 SF) (ITE 934) | | | | | | | |
| Baseline Trips | 1,602 | 61 | 58 | 119 | 59 | 54 | 113 |
| Total New Trips after Pass-by | 722 | 31 | 28 | 59 | 27 | 24 | 51 |
| Proposed Land Use- McDonald's (4,070 SF) (ITE 934) | | | | | | | |
| Baseline Trips | 1,824 | 69 | 66 | 135 | 67 | 62 | 129 |
| Total New Trips after Pass-by | 822 | 35 | 32 | 67 | 30 | 28 | 58 |
| Net Difference in Total Trips | +100 | +4 | +4 | +8 | +3 | +4 | +7 |

As shown in the table and based on ITE Trip Generation calculations, the proposed McDonald's is expected to generate approximately 822 weekday daily new trips, with 67 of these new trips occurring during the morning peak hour and 58 of these trips occurring during the afternoon peak hour. In comparison, the previously existing Burger King is calculated to previously be generating approximately 722 weekday daily new trips, with 59 and 51 of these new trips occurring during the morning and afternoon peak hour, respectively. Based on this analysis and trips being calculated based on building area, the proposed fast-food restaurant is calculated to generate 100 additional daily new trips, eight (8) more trips in the morning peak hour, and seven (7) more trips during the afternoon peak hour compared to the previously existing fast-food restaurant in the same development area.

CONCLUSIONS

The current proposal, although the same land use type, is calculated to generate 100 additional daily new trips, eight (8) more new trips in the morning peak hour, and seven (7) more new trips during the afternoon peak hour compared to the previously existing fast-food restaurant in the same development area. The increase in trips is due to the building area increasing by 496 square feet and building area being the variable utilized in calculating trip generation for fast-food restaurants. Based on the land use remaining the same, it is conceivable that two fast-food restaurants with similar building areas could generate the same number of trips independent of building areas. Regardless, even if it is believed that trips could increase based on building area, the increase of eight (8) new trips during the peak hour equates to approximately one additional trip every seven (7) minutes. Kimley-Horn and Associates believes the project will be successfully incorporated into the existing roadway network.

If you have any questions or require anything further, please feel free to call me at (720) 943-9962.

Sincerely,

KIMLEY-HORN AND ASSOCIATES, INC.



Jeffrey R. Planck, PE
Project Traffic Manager





Trip Generation Worksheets

Existing Burger King Daily Trip Generation Calculations

| | TRIP GENERATION CHARACTERISTICS | | | | | DIRECTIONAL DISTRIBUTION | | BASELINE TRIPS | | | PASS-BY CAPTURE | | NEW EXTERNAL VEHICLE TRIPS | | |
|---------------|--|-------------|-------|----------|-----------------|--------------------------|-----------|----------------|-----|-------|-----------------|----------|----------------------------|-----|-------|
| | Land Use + Code | Source | Scale | ITE Unit | Equation/Rate | Entering % | Exiting % | In | Out | Total | Rate | PB Trips | In | Out | Total |
| 1 | (934) Fast-Food Restaurant with Drive-Through Window | ITE 12th Ed | 3.57 | KSF | $T = 448.12(X)$ | 50% | 50% | 801 | 801 | 1,602 | 55.0% | 881 | 360 | 361 | 721 |
| Total: | | | | | | | | 801 | 801 | 1,602 | 55.0% | 881 | 360 | 361 | 721 |

Existing Burger King AM Peak Hour Trip Generation Calculations

| | TRIP GENERATION CHARACTERISTICS | | | | | DIRECTIONAL DISTRIBUTION | | BASELINE TRIPS | | | PASS-BY CAPTURE | | NEW EXTERNAL VEHICLE TRIPS | | |
|---------------|--|-------------|-------|----------|----------------|--------------------------|-----------|----------------|-----|-------|-----------------|----------|----------------------------|-----|-------|
| | Land Use + Code | Source | Scale | ITE Unit | Equation/Rate | Entering % | Exiting % | In | Out | Total | Rate | PB Trips | In | Out | Total |
| 1 | (934) Fast-Food Restaurant with Drive-Through Window | ITE 12th Ed | 3.57 | KSF | $T = 33.24(X)$ | 51% | 49% | 61 | 58 | 119 | 50.0% | 60 | 31 | 28 | 59 |
| Total: | | | | | | | | 61 | 58 | 119 | 50.0% | 60 | 31 | 28 | 59 |

Existing Burger King PM Peak Hour Trip Generation Calculations

| | TRIP GENERATION CHARACTERISTICS | | | | | DIRECTIONAL DISTRIBUTION | | BASELINE TRIPS | | | PASS-BY CAPTURE | | NEW EXTERNAL VEHICLE TRIPS | | |
|---------------|--|-------------|-------|----------|---------------|--------------------------|-----------|----------------|-----|-------|-----------------|----------|----------------------------|-----|-------|
| | Land Use + Code | Source | Scale | ITE Unit | Equation/Rate | Entering % | Exiting % | In | Out | Total | Rate | PB Trips | In | Out | Total |
| 1 | (934) Fast-Food Restaurant with Drive-Through Window | ITE 12th Ed | 3.57 | KSF | $T = 31.6(X)$ | 52% | 48% | 59 | 54 | 113 | 55.0% | 62 | 27 | 24 | 51 |
| Total: | | | | | | | | 59 | 54 | 113 | 55.0% | 62 | 27 | 24 | 51 |

Proposed McDonald's Daily Trip Generation Calculations

| TRIP GENERATION CHARACTERISTICS | | | | | | DIRECTIONAL DISTRIBUTION | | BASELINE TRIPS | | | PASS-BY CAPTURE | | NEW EXTERNAL VEHICLE TRIPS | | |
|---------------------------------|--|-------------|-------|----------|-----------------|--------------------------|-----------|----------------|-----|-------|-----------------|----------|----------------------------|-----|-------|
| | Land Use + Code | Source | Scale | ITE Unit | Equation/Rate | Entering % | Exiting % | In | Out | Total | Rate | PB Trips | In | Out | Total |
| 1 | (934) Fast-Food Restaurant with Drive-Through Window | ITE 12th Ed | 4.07 | KSF | $T = 448.12(X)$ | 50% | 50% | 912 | 912 | 1,824 | 55.0% | 1,003 | 410 | 411 | 821 |
| Total: | | | | | | | | 912 | 912 | 1,824 | 55.0% | 1,003 | 410 | 411 | 821 |

Proposed McDonald's AM Peak Hour Trip Generation Calculations

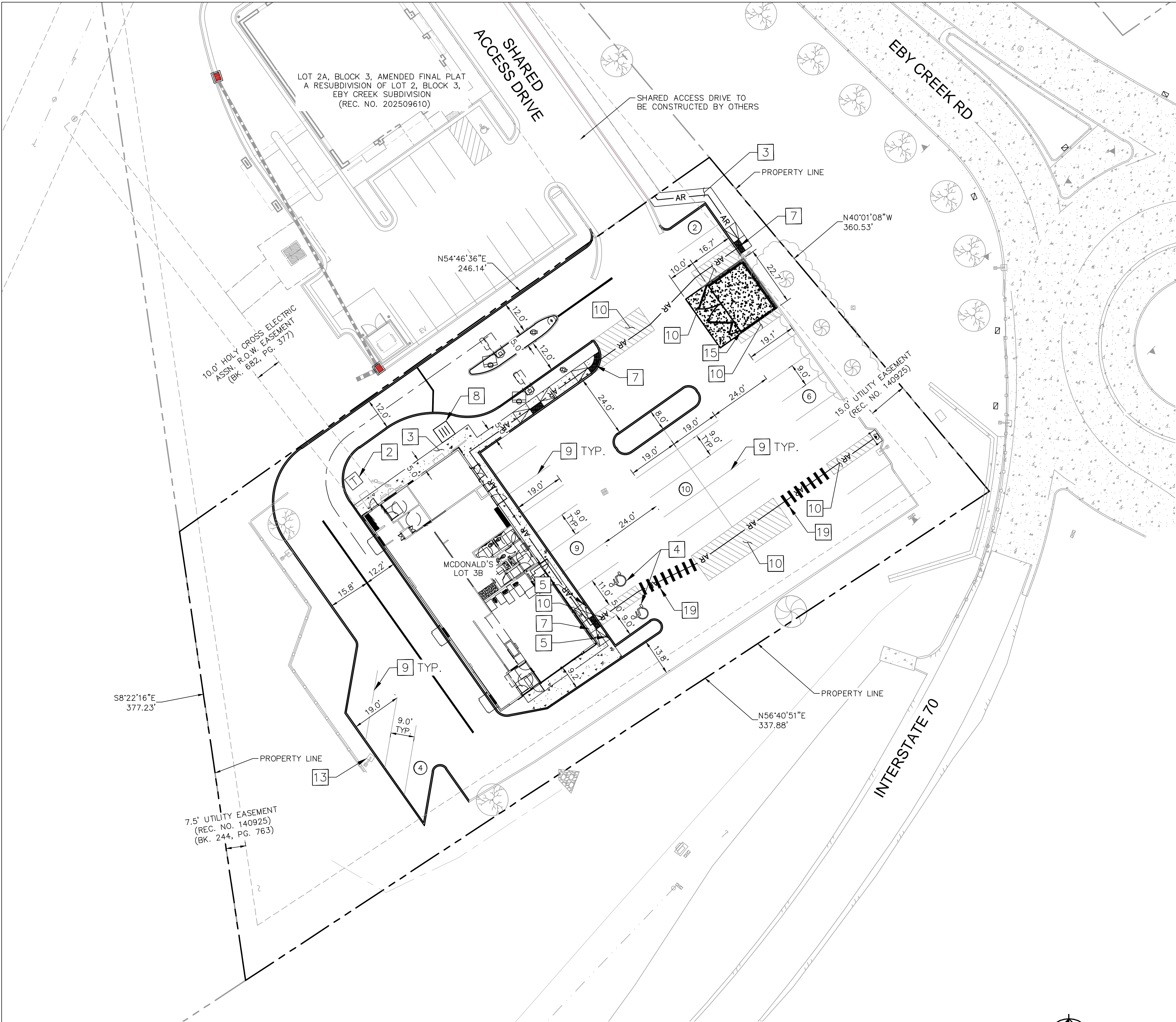
| TRIP GENERATION CHARACTERISTICS | | | | | | DIRECTIONAL DISTRIBUTION | | BASELINE TRIPS | | | PASS-BY CAPTURE | | NEW EXTERNAL VEHICLE TRIPS | | |
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| | Land Use + Code | Source | Scale | ITE Unit | Equation/Rate | Entering % | Exiting % | In | Out | Total | Rate | PB Trips | In | Out | Total |
| 1 | (934) Fast-Food Restaurant with Drive-Through Window | ITE 12th Ed | 4.07 | KSF | $T = 33.24(X)$ | 51% | 49% | 69 | 66 | 135 | 50.0% | 68 | 35 | 32 | 67 |
| Total: | | | | | | | | 69 | 66 | 135 | 50.0% | 68 | 35 | 32 | 67 |

Proposed McDonald's PM Peak Hour Trip Generation Calculations

| TRIP GENERATION CHARACTERISTICS | | | | | | DIRECTIONAL DISTRIBUTION | | BASELINE TRIPS | | | PASS-BY CAPTURE | | NEW EXTERNAL VEHICLE TRIPS | | |
|---------------------------------|--|-------------|-------|----------|---------------|--------------------------|-----------|----------------|-----|-------|-----------------|----------|----------------------------|-----|-------|
| | Land Use + Code | Source | Scale | ITE Unit | Equation/Rate | Entering % | Exiting % | In | Out | Total | Rate | PB Trips | In | Out | Total |
| 1 | (934) Fast-Food Restaurant with Drive-Through Window | ITE 12th Ed | 4.07 | KSF | $T = 31.6(X)$ | 52% | 48% | 67 | 62 | 129 | 55.0% | 71 | 30 | 28 | 58 |
| 2 | | | | | | | | | | | | | | | |
| Total: | | | | | | | | 67 | 62 | 129 | 55.0% | 71 | 30 | 28 | 58 |



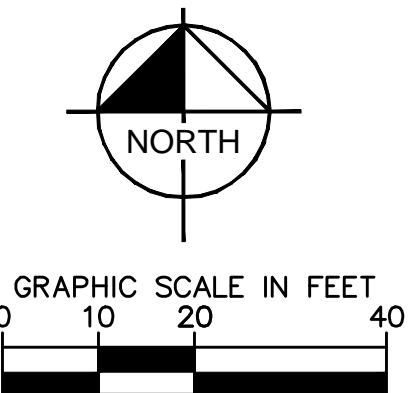
Conceptual Site Plan



| LEGEND | |
|--------|---|
| | PROPERTY LINE |
| | SETBACK |
| | ACCESSIBLE ROUTE PARKING COUNT |
| | PROPOSED LANDSCAPE |
| | PROPOSED CONCRETE STANDARD DUTY ASPHALT PAVEMENT |
| | EXISTING PROPERTY LINE |
| | EXISTING RIGHT OF WAY |
| | EXISTING EASEMENT |
| | EXISTING CONCRETE |
| | EXISTING UNDERGROUND ELECTRIC |
| | EXISTING WATER LINE |
| | EXISTING WATER SERVICE |
| | EXISTING FIRE HYDRANT |
| | EXISTING COMMUNICATIONS |
| | EXISTING OVERHEAD ELECTRIC |
| | EXISTING ELECTRICAL EQUIPMENT |
| | EXISTING GAS |
| | EXISTING STREET LIGHT |
| | EXISTING STORM SEWER |
| | EXISTING STORM MANHOLE |
| | EXISTING STORM CURB INLET |
| | EXISTING SANITARY SEWER |
| | 6" SANITARY SERVICE (BY MASTER DEVELOPER) |
| | 4" SANITARY SERVICE (BY MASTER DEVELOPER) |
| | EXISTING SANITARY MANHOLE |
| | PROPOSED ELECTRIC LINE |
| | PROPOSED TRANSFORMER |
| | PROPOSED COMMUNICATIONS LINE |
| | PROPOSED GAS LINE |
| | PROPOSED GREASE INTERCEPTOR |
| | PROPOSED SANITARY SEWER SERVICE LINE |
| | PROPOSED DOMESTIC WATER SERVICE LINE |
| | PROPOSED STORM SEWER |
| | PROPOSED FLUSH CURB |
| | PROPOSED STANDARD CURB & GUTTER |
| | PROPOSED CURB WITH SPILL GUTTER |
| | PROPOSED CURB CUTS (CURB LINE TO HAVE 1' CURB CUTS APPROXIMATELY EVERY 10') |
| | PROPOSED CURB INLET |
| | PROPOSED STORM MANHOLE |
| | PROPOSED GRATED INLET |
| | PROPOSED SITE LIGHT |
| | PROPOSED RIPRAP |
| | PROPOSED BOTTOM OF RAIN GARDEN |
| | PROPOSED TOP OF RAIN GARDEN |
| | PROPOSED ARCHITECTURAL WALL |
| | EXISTING STREAM |
| | EXISTING WETLAND |
| | EXISTING FEMA FLOOD ZONE AE |
| | WETLAND MITIGATION AREA |
| | WETLAND IMPACT AREA |

- KEY NOTES**
- PROPOSED STANDARD CURB AND GUTTER PER CITY OF GRAND JUNCTION STANDARD DETAIL C-05
 - PROPOSED TRANSFORMER LOCATION
 - PROPOSED CONCRETE SIDEWALK (DIMENSION PER PLAN)
 - PROPOSED ADA SYMBOL (REF. SHEET C1.5 FOR DETAIL)
 - PROPOSED ADA SIGN (REF. SHEET C1.5 FOR DETAIL)
 - PROPOSED 6" WIDE X 6" TALL CURB HEAD (REF. SHEET C1.5 FOR DETAIL)
 - PROPOSED ADA RAMP
 - PROPOSED BIKE RACK (3 SPACES) (REF. SHEET C1.5 FOR DETAIL)
 - PROPOSED 4" WIDE PARKING STRIPE
 - PROPOSED 4" WIDE DIAGONAL PAVEMENT MARKINGS AT 36" ON CENTER
 - PROPOSED LANDSCAPING (REF. LANDSCAPING PLANS)
 - PROPOSED MONUMENT SIGN
 - PROPOSED SITE LIGHT (REF. LIGHTING PLANS FOR DETAILS)
 - PROPOSED TRASH ENCLOSURE (FRONT LOADING DUMPSTER) (REF. SHEETS S1.10-S1.30 FOR DETAILS)
 - PROPOSED CROSSWALK (REF. SHEET C1.5 FOR DETAIL)

- GENERAL NOTES**
- ALL EXISTING UTILITIES TO REMAIN UNLESS OTHERWISE NOTED.
 - UTILITIES TO BE REMOVED ARE TO BE CAPPED IN PLACE IN ACCORDANCE WITH LOCAL UTILITY REGULATIONS.



ACCEPTANCE BLOCK

THE TOWN OF EAGLE REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE CITY'S DEVELOPMENT STANDARDS, SUBJECT TO THESE PLANS BEING SEALED, SIGNED, AND DATED BY THE PROFESSIONAL OF RECORD. REVIEW BY THE CITY DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN. THE CITY NEITHER ACCEPTS NOR ASSUMES ANY LIABILITY FOR ERRORS OR OMISSIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE PROFESSIONAL OF RECORD.

CONSTRUCTION MUST COMMENCE WITHIN ONE YEAR FROM THE DATE OF PLAN SIGNATURE.

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